

Lincoln Avenue Specific Plan

Proposed Specific Plan

June 9, 2021



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Revised and new text is highlighted in blue. Photos and illustrations have been updated. Certain sections have been reorganized.

Compared to draft plan dated April 28, 2021

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Ch. 1

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Introduction

Since serving some of Pasadena's earliest subdivisions in the late nineteenth century, Lincoln Avenue has remained one of the City's important corridors, providing retail, housing, industrial services, and cultural and educational institutions to the residents of Pasadena. However, Lincoln Avenue's rich history is not without struggle; discriminatory housing policy, freeway construction, and a presence of some heavy industry stifled the area's recovery from the Great Depression, and contributed to the corridor's fragmentation and isolation from the city center throughout the mid-twentieth century. Despite these challenges, Lincoln Avenue's diverse and resilient community has steadfastly supported local businesses and institutions, and initiated various plans, programs, and improvements which continue today. This Specific Plan presents an opportunity for these efforts to be grounded in policy and practice, and for the City and community to work together in confronting planning challenges and envisioning the future of Lincoln Avenue. **The Specific Plan establishes the following vision statement for Lincoln Avenue, which reflects ideas and feedback from a multi-phase community engagement effort between 2018 and 2021:**

Lincoln Avenue will be the heart of the greater residential neighborhood where commercial and multifamily uses co-exist in a walkable, vibrant environment with public realm improvements that create a cohesive and well-connected corridor.

In addition to an extensive public outreach and visioning process, the Specific Plan represents the outcome of a robust technical planning and design effort, directly informed by the perspectives and expertise of community members, City staff, the Planning Commission, Design Commission, Northwest Commission, and the City Council. This chapter introduces the Lincoln Avenue Specific Plan project area and purpose, the Specific Plan's relationship to other City planning documents, an overview of the planning and outreach process, and the organization of the Specific Plan document.

CHAPTER OVERVIEW

This chapter is organized into the following sections:

- » **1.1 Specific Plan Area**
- » **1.2 Purpose**
- » **1.3 Our Pasadena Program**
- » **1.4 Relationship to Other Planning Documents**
- » **1.5 Planning Process and Outreach**
- » **1.6 Specific Plan Organization**

WHAT IS A SPECIFIC PLAN?

In the State of California, a Specific Plan is a regulatory tool that local governments use to implement their General Plan and to guide development in a localized area. While the General Plan is the primary guide for growth and development in a community, a Specific Plan is able to focus on the unique characteristics of a special area by customizing the land use regulations and development standards for that area. A Specific Plan establishes a link between the policies and implementation programs in the General Plan and individual development proposals in a defined area within the city.



Palm Plaza and public realm with sculptural marker on Lincoln Ave. near Montana St.

1.1 Specific Plan Area

Map 1.1-1: Lincoln Avenue Specific Plan Area



-  Parcel Lines
-  Proposed Specific Plan Boundary

Lincoln Avenue is an important corridor in Pasadena, with a backdrop of the San Gabriel Mountains to the north. The Specific Plan area generally consists of properties fronting the 1.1-mile section of Lincoln Avenue between Hammond Street on the south and the northern City boundary, just north of Montana Street.

As shown in Map 1.1-1, the Lincoln Avenue Specific Plan area includes the following parcels:

- » All parcels with frontage on Lincoln Avenue, between Hammond Street and the northern City boundary, just north of Montana Street.
- » Select parcels adjacent to those fronting Lincoln Avenue between Hammond Street, the I-210 Freeway and the northern City boundary, excluding John Muir High School.

While surrounding neighborhoods to the east, west, and south are outside the Plan area, these adjacent communities help to inform policies and standards in the plan and are intended to benefit from the Specific Plan's implementation. For example, John Muir High School, while located outside of the Specific Plan area, functions as a visual anchor and focal point within the corridor, and influences the surrounding environment.



Retail and services businesses on the east side of Lincoln Ave. between Montana St. and Wyoming St.



Multi-family housing with setback and parkway on the northwest corner of Lincoln Ave. and Zanja St.

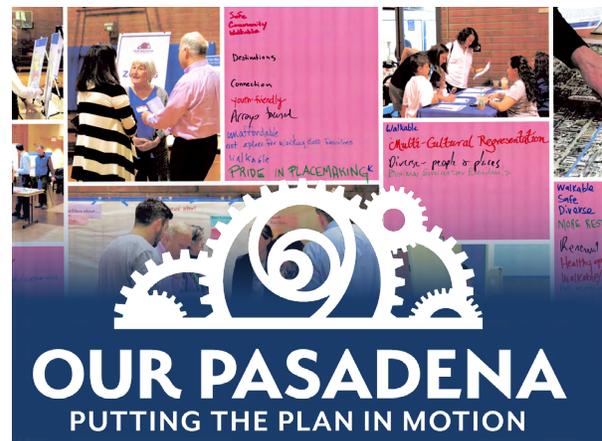
1.2 Purpose

The purpose of the Lincoln Avenue Specific Plan is to facilitate and encourage development and improvements that help realize the community's vision for Lincoln Avenue. The Plan optimizes land uses to increase opportunities for financially feasible commercial and residential developments, and helps ensure that new buildings, sidewalk improvements, and added amenities contribute positively to the pedestrian experience. The Plan includes standards to protect historic resources and existing elements of the area that provide a sense of place and distinct character, and to prevent and mend disjointed development, which can occur in the absence of a comprehensive planning effort.

All of the parcels in the Plan area are developed with commercial, residential, or industrial uses, so future redevelopment opportunities will focus on infill development and making incremental changes to the district's built environment and land use mix. This document will be used by property and business owners, grant-seeking nonprofits, developers, decision makers, and City staff as the regulations that will guide private and public development projects. While the Specific Plan introduces a framework and toolkit for designing and implementing future developments, it does not mandate or accelerate any specific projects or immediate changes to the built environment.

1.3 Our Pasadena Program

The General Plan is a document that outlines the community's vision for Pasadena over the next 20 years. As an overall visioning document, the General Plan's goals and policies are implemented in various ways, including Specific Plans. *Our Pasadena – Putting the Plan in Motion* is the City's General Plan implementation program, focused on updating Pasadena's Zoning Code and establishing neighborhood-specific design and land-use goals for the City's eight Specific Plans: Central District, East Colorado, East Pasadena, Fair Oaks/Orange Grove, Lamanda Park, Lincoln Avenue, North Lake, and South Fair Oaks.



SUMMARY OF 2015 GENERAL PLAN GUIDING PRINCIPLES

1. Growth will be targeted to serve community needs and enhance the quality of life.
2. Pasadena's historic resources will be preserved.
3. Pasadena will be an economically vital city by providing jobs, services, revenues, and opportunities.
4. Pasadena will be a socially, economically, and environmentally sustainable community.
5. Pasadena will be a city where people can circulate without cars.
6. Pasadena will be a cultural, scientific, corporate, entertainment and education center for the region.
7. Community participation will be a permanent part of achieving a greater city.
8. Pasadena is committed to public education and a diverse educational system responsive to the broad needs of the community.

Pasadena General Plan Land Use Element, 2015

1.4 Relationship to Other Planning Documents

GENERAL PLAN

The Lincoln Avenue Specific Plan is one of eight Specific Plans that serve to implement the goals and policies of the City's 2015 General Plan Land Use and Mobility Elements. The General Plan contains eight Guiding Principles and a series of goals and policies that demonstrate the relationship between land use and high quality design, the arts and culture, sustainable infrastructure, a vital economy, exemplary public services, and public involvement and participation.

The Specific Plan mirrors and builds upon the General Plan's policies to achieve consistency with the General Plan's vision and guidance. The Specific Plan seeks to stimulate economic development, support pedestrian mobility, target housing and employment opportunities on Lincoln Avenue in a contextually sensitive manner, and consistently provide access for residents and local businesses to participate in governance and actively engage in the community. Projects that are consistent with the Specific Plan policies and development standards will in turn be consistent with the General Plan policies and Guiding Principles.

While the Specific Plan establishes an overall vision for Lincoln Avenue, the standards and guidelines herein apply specifically to private development and adjacent sidewalk. The Specific Plan serves as one of many complementary tools the City uses to implement the General Plan and Specific Plan visions, and to meet larger sustainability goals through prioritization and guidance for private and public investments. The goals and primary areas of focus for these various tools are briefly summarized in this section.

ZONING CODE

The Specific Plan will provide the policy and guidance for all development, public and private, that occurs within the Specific Plan area. An ordinance will be codified into the Zoning Code that will regulate allowable land uses, development standards, and all other elements of the Specific Plan that are geared toward regulation of private development.

PREVIOUS LINCOLN AVENUE SPECIFIC PLAN (2013)

As the 2013 Lincoln Avenue Specific Plan was adopted to implement the 1994 General Plan update, the 2015 General Plan requires an updated implementation document for each of Pasadena's eight Specific Plan areas. While the new Specific Plan builds on many of the objectives from the 2013 Specific Plan, this document replaces and supersedes the previous Specific Plan, introducing updated policies and standards that address current and future community needs, opportunities, and challenges.

GENERAL PLAN GOAL 35: LINCOLN AVENUE

"A vibrant neighborhood oriented district, with new housing options and a complement of local-serving retail and service businesses, office spaces, and community uses, all tied together with public improvements that create a vibrant and enjoyable pedestrian environment."

GENERAL PLAN POLICIES

- » **35.1 Neighborhood Core.** Create a neighborhood "main street" that will serve as the focal point for the neighborhoods surrounding Lincoln Avenue.
- » **35.2 Residential Neighborhoods.** Preserve and enhance existing residential areas.
- » **35.3 New Housing.** Provide new opportunities for all types of housing along the corridor including multi-family housing and mixed-use residential-commercial developments in its core that are located, scaled, and designed to assure compatibility with and preserve neighboring single-family residential uses.
- » **35.4 Catalytic Projects.** Facilitate opportunities for catalytic developments that provide desired neighborhood-oriented retail and service businesses, local employment opportunities, and a link to the community.
- » **35.5 Re-Use of Industrial Properties.** Provide for the gradual phasing out of industrial uses that create conflicts with surrounding neighborhoods.
- » **35.6 Quality Design.** Support design that contributes to the enhanced character of the City and Northwest Pasadena in particular.
- » **35.7 Pedestrian Environment.** Enrich the pedestrian environment along Lincoln Avenue through well designed and appropriately scaled projects and pleasing streetscapes with wide sidewalks, street trees and signage that helps to establish the Lincoln Avenue identity.
- » **35.8 Investment.** Encourage investment, maintenance, and pride in the Lincoln Avenue Specific Plan area.
- » **35.9 A Safe Neighborhood.** Enhance public safety.
- » **35.10 Mobility Choices.** Redesign Lincoln Avenue to accommodate a mix of mobility choices including walking, bicycling, and transit in addition to the automobile.

Pasadena General Plan Land Use Element, 2015

DESIGN GUIDELINES FOR NEIGHBORHOOD COMMERCIAL & MULTIFAMILY DISTRICTS

The Design Guidelines for Neighborhood Commercial & Multi-Family Districts (October 2009) supplement the General Plan Land Use Element design related goals and policies, and offer more direction for proceeding with the design of a project. The guidelines illustrate options, solutions, and techniques to achieve the goal of excellence in new design specifically for commercial, residential, and mixed use buildings that are subject to design review. The guidelines are not zoning regulations or development standards, but rather performance goals that apply to areas within the City that do not have detailed guidelines or supplement existing guidelines, including those included in this Specific Plan.

SIGN DESIGN GUIDELINES

The Sign Design Guidelines provide guidance in the way signs are designed, constructed, and placed in order to further implement the purposes of Chapter 17.72 (Sign Regulations) of the City of Pasadena's Municipal Code. The guidelines are intended to provide good examples of techniques that should be used in order to meet the City's expectations for quality business signage to be applied during the City's design review process or the approval of a discretionary land use permit.

PASADENA PEDESTRIAN PLAN

The Department of Transportation's Pasadena Pedestrian Plan (July 2006) provides guidance to preserve the walkability of pedestrian areas, better design and develop pedestrian-friendly projects, better integrate pedestrian improvements into street maintenance and traffic management programs, and implement public education and enforcement programs that improve pedestrian safety and increase levels of walking. An update to the Pasadena Pedestrian Plan is currently under development at the time of writing this Specific Plan through the *Pasadena Walks!* project, designed to inform the plan development through identifying barriers to walking, potential improvements, and locations to prioritize based on analyses, public input, and focus groups. This Specific Plan reinforces pedestrian-friendly design and development in new projects through land use, development standards, and design guidelines.

BICYCLE TRANSPORTATION ACTION PLAN

The Department of Transportation's Bicycle Transportation Action Plan (BTAP) (August 2015) provides specific goals, objectives, actions, and timelines for creating an environment (1) where people circulate without a car, (2) that significantly increases the number of people who commute by bike, (3) that increases the number of people who use a bike for utilitarian trips, fitness and recreation,

and (4) that provides business and economic benefits for the City. The plan provides details for a network of bikeways so that every neighborhood is within 1/4 mile of an effective bicycle route and funding strategies to implement the plan. This Specific Plan supports the BTAP through promoting enhanced sidewalks that allocate space for bicycle parking and supporting bicycle infrastructure within the plan area. While none of the bike improvements currently prioritized in the BTAP are located within the Lincoln Avenue Specific Plan area, this plan suggests and prioritizes the need to connect Lincoln to the larger Pasadena community.

PASADENA STREET DESIGN GUIDE

The Department of Transportation's Pasadena Street Design Guide (March 2017) implements the 2015 General Plan Mobility Element complete streets policy, including the following goals and objectives:

- » Streets should reflect neighborhood character and accommodate all users.
- » Complete Streets: Streets should accommodate all users such as pedestrians, bicyclists, public transit, skateboarders and scooters.
- » Streets should reflect individual neighborhood character and needs, and support healthy activities such as walking and bicycling.

This Specific Plan references the Street Design Guide as it applies to sidewalks, parkways, and street trees, which fall under Chapter 5 (Public Realm Standards and Design Guidelines) of the Specific Plan.

MASTER STREET TREE PLAN

The Department of Public Works' Master Street Tree Plan serves as the guiding document that designates the official tree species to be planted on a block-by-block basis throughout the City. This Specific Plan references the Master Street Tree Plan in Appendix A.2 (Design Guidance for Tree Selection) to guide discussions between the City and community when updating the Master Street Tree Plan for the area. The appendix includes a description of the existing street trees along Lincoln Avenue within the Specific Plan area, followed by recommendations for potential new species.

PASADENA CLIMATE ACTION PLAN

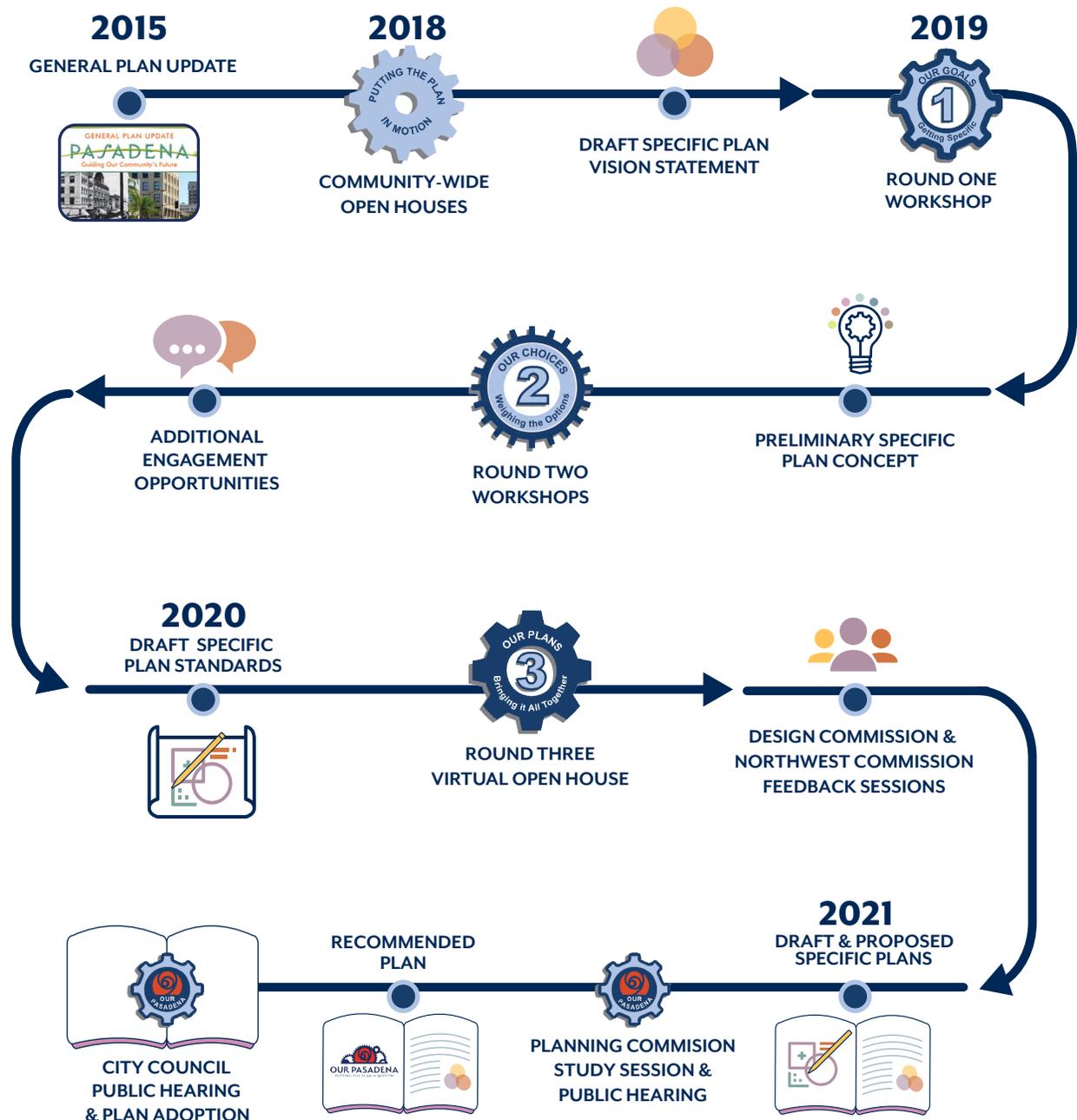
The Pasadena Climate Action Plan (CAP) (March 2018) provides a strategic framework for measuring, planning, and reducing the City's share of greenhouse gas (GHG) emissions with the goal of reducing emissions by more than half by the year 2035. This Specific Plan supports the CAP and the identified strategies to reduce GHG through sustainable land use and pedestrian infrastructure as well as urban greening, which are addressed in Chapter 4 (Land Use), Chapter 5 (Public Realm), and Appendix A.2 (Design Guidance for Tree Selection).

1.5 Planning Process and Outreach

The Lincoln Avenue Specific Plan is informed by a thorough planning and public outreach process led by the City’s Planning & Community Development Department, and supported by a consultant team of urban planners, urban designers, outreach specialists, economists, architects, and landscape architects. The planning process also involved coordination with staff from Pasadena’s departments of Public Works, Transportation, Economic Development, Northwest Programs, and Accessibility Coordinator.

1.5.1 PUBLIC OUTREACH

Throughout the planning process, the City solicited input from residents, property owners, businesses, community leaders, and other stakeholder groups through a variety of outreach events, public meetings, and online engagement tools. The following list provides a summary of public outreach methods, workshops, meetings, and hearings, and an overview of key recurring feedback themes from the outreach process.



Online Survey

As the first public outreach activity, this online survey provided an initial opportunity to gather thoughts about Lincoln Avenue from community members and others familiar with the area, including positive and negative elements of building design and development, the pedestrian environment, and other community planning issues. The City used the survey responses to begin identifying places of interest and topics for discussion in the Round 1 Workshop. The online survey also served as a way to introduce the community to the Specific Plan process and spread awareness of the Our Pasadena General Plan implementation program.

Round 1 Workshop

June 14, 2018

In the Round 1 Workshop, the Planning & Community Development Department facilitated a listening and learning session to find out more about participants' experience living and/or working in, or visiting the Lincoln Avenue area, and their ideas for how the Specific Plan could improve the area. The workshop began with a brief interactive visioning activity, and an introductory presentation on the Specific Plan Update program, the General Plan vision for the plan area, and background information. For the majority of the workshop, participants were divided into small groups with facilitated discussions on specific topics such as appropriate land uses and housing types, streetscape and public amenities, and mobility and parking. The main objective of the workshop was to solicit feedback from the community, rather than reach consensus on any particular topic. This workshop was followed by a second online survey.

Virtual Walking Tour

February 2, 2019

Though originally scheduled as an outside walking tour of the area, due to heavy rains, City staff and community members did a virtual walking tour at John Muir High School. The virtual tour helped to gather participants' observations and experiences throughout the intended route of the walking tour. Participants wrote comments on walking tour worksheets/guides as staff navigated through portions of Lincoln Avenue, beginning at the intersection of Wyoming and Lincoln and ending at John Muir High School. At the end of the tour, participants identified specific locations on large maps with image cards depicting desired uses.



Round 1 Workshop



Virtual Walking Tour



Round 2 Workshop

Round 2 Workshop

February 28, 2019

The Planning & Community Development Department hosted a second community workshop to present preliminary land use and urban form concepts that considered a number of factors, including community feedback received since the first workshop. Input received helped to refine these concepts and guide the drafting of goals, policies, and development standards. In an opening icebreaker activity, participants were able to reaffirm what we heard so far from the community by placing stickers next to those comments. Next, City staff gave a PowerPoint presentation covering background information on the program, an overview of the existing Specific Plan area, emerging themes and draft vision, and preliminary concepts. Lastly, participants broke out into small groups to discuss the preliminary concepts. Each table reported back to the larger group with a summary of main points.

Youth Summit

October 19, 2019

The Planning & Community Development Department hosted an OurPasadena Youth Summit for Pasadena High School students at the Robinson Recreation Center. Through the use of multi-media tools and interactive activities, the event introduced city planning to youth and allowed participants to share their unique perspective on what they think will make Pasadena a better place now and in the future.



Youth Summit

WHAT WE HEARD

Participants shared a wide range of input throughout the outreach process, including the following recurring themes:

- » Strengthen community identity through placemaking and cohesive design
- » Address the lack of open space by providing more walkable gathering spaces suitable for families and pets
- » Create a more pleasant environment to stroll around the neighborhood and to the Arroyo Seco
- » Provide more landscaping and street trees that provide shade and relief from harsh weather
- » Strengthen the connection between the northern and south parts of the plan area through public art and wayfinding signage
- » Maintain low-density character with 2- to 3-story scale
- » Support a business environment in which “mom and pop” shops and local retailers can thrive
- » Create opportunities for affordable, family-sized homes
- » Support clean industry, but beautify buildings with landscaped setbacks
- » Continue to limit liquor stores and alcohol sales
- » Make the Lincoln Avenue corridor safer and more friendly for bicyclists

Round 3 Virtual Open House

August - April 2021

For the third and final round of community workshops, the Planning & Community Development Department hosted an interactive virtual open house website and live webinar on August 27, 2020. Through an introductory presentation and a series of informational materials, staff presented the refined Lincoln Avenue Specific Plan vision and concept, along with full draft standards for the Land Use, Public Realm, and Development & Design chapters of the plan. Participants were encouraged to provide detailed input through an online survey, and to submit questions in the Q&A portion of the live webinar event. While the community was unable to gather in person due to the COVID-19 pandemic, the virtual platform was available 24/7 for an extended period of time, allowing participants to visit and provide feedback at their pace and convenience, as well as download materials and share the open house site with family, friends, and neighbors.



Round 3 Workshop - Virtual Open House Website

Northwest Commission & Design Commission Meetings

November 10 & December 22, 2020

Following the Round 3 Webinar, the Planning & Community Development Department presented at the Northwest Commission and Design Commission meetings in Winter of 2020 to solicit feedback on the draft Lincoln Avenue Specific Plan, respond to clarifying questions from commissioners, and discuss various issues to be considered in the development of standards, policies, and implementation strategies for the next draft of the plan.

Council District 1 Community Meeting

March 10, 2021

The Planning & Community Development Department presented to community members from Council District 1 about the Lincoln Avenue Specific Plan process and draft development standards. Following the presentation, staff facilitated a Q&A session with attendees and fielded comments on the draft plan.

Summary of Mailings & Promotional Materials

In addition to these public outreach events and workshops, the Planning & Community Development Department has advertised the Lincoln Avenue Specific Plan and provided program updates through the following platforms and publications:

- » Mailers to property owners, occupants, and renters within the plan area and within 500 feet of the plan area
- » E-mailing list for program newsletter subscribers
- » OurPasadena Program website
- » OurPasadena and Citywide social media accounts
- » Council District Newsletters
- » City of Pasadena InFocus
- » Local press coverage

Planning Commission Meetings

Section to be completed after Planning Commission meetings

City Council Meeting

Section to be completed after City Council meeting

1.5.2 ENVIRONMENTAL CLEARANCE

In the 2015 Pasadena General Plan update, the City prepared a programmatic General Plan Environmental Impact Report (GP EIR) to analyze potential citywide impacts, broad policy alternatives, and programmatic mitigation measures associated with the update of the General Plan and specific plan amendments. An Addendum to the GP EIR was prepared for this Specific Plan to address potential site-specific environmental impacts associated with the update to the Lincoln Avenue Specific Plan.

Per the GP EIR, future discretionary review may rely on analysis provided in the GP EIR for the purpose of tiering and/or streamlining. The purpose of tiering is to use the analysis of general matters contained in a broader EIR (such as the GP EIR) with later California Environmental Quality Act (CEQA) documents on narrower or more site specific projects. Tiering serves to reduce repetitive analysis and provide subsequent site specific analysis at a time when it is meaningful. Tiering is common and appropriate when the sequence of analysis is from a General Plan EIR to a program of lesser scope, such as a specific plan. Therefore, CEQA review required for this Specific Plan may tier from the GP EIR pursuant to CEQA Guidelines Section 15152.

1.6 Specific Plan Organization

The Specific Plan contains seven chapters, which are organized in the following manner:

CH. 1 - INTRODUCTION

This chapter presents the Specific Plan's purpose, and outlines the planning and outreach process. This chapter also discusses the plan's relationship to other planning documents, and introduces the 2015 General Plan principles, goals, and policies that inform the Lincoln Avenue Specific Plan.

CH. 2 - BACKGROUND & CONTEXT

This chapter provides additional historical context for the plan area and identifies challenges and opportunities within Lincoln Avenue's existing conditions.

CH. 3 - VISION, GOALS & POLICIES

This chapter establishes the overall vision for the Lincoln Avenue Specific Plan area, and specific visions for the sub-areas north and south of the I-210 Freeway. The vision is followed by goals and policies organized by subarea and topic.

CH. 4 - LAND USE

This chapter introduces the Specific Plan's updated zoning districts, and establishes the types of land uses allowed for potential new development within each zoning district.

CH. 5 - PUBLIC REALM STANDARDS

This chapter presents standards and guidelines for the public realm adjacent to new development, including sidewalks, parkways, street trees, and paseos.

CH. 6 - DEVELOPMENT STANDARDS & DESIGN GUIDELINES

This chapter presents standards and guidelines for development of private property. Standards in this chapter are organized into five sections, including Intensity & Density, Massing, Ground Floor, Open Space, and Access & Parking.

CH. 7 - IMPLEMENTATION & ADMINISTRATION

This chapter presents implementation actions and responsibilities, and potential programming and funding opportunities to bring the Specific Plan vision to life.



Lincoln Avenue Specific Plan area
 Imagery ©2021 Google, Imagery ©2021 Maxar Technologies, U.S. Geological Survey, USDA Farm Service Agency, Map data ©2021 Google

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Ch. 2

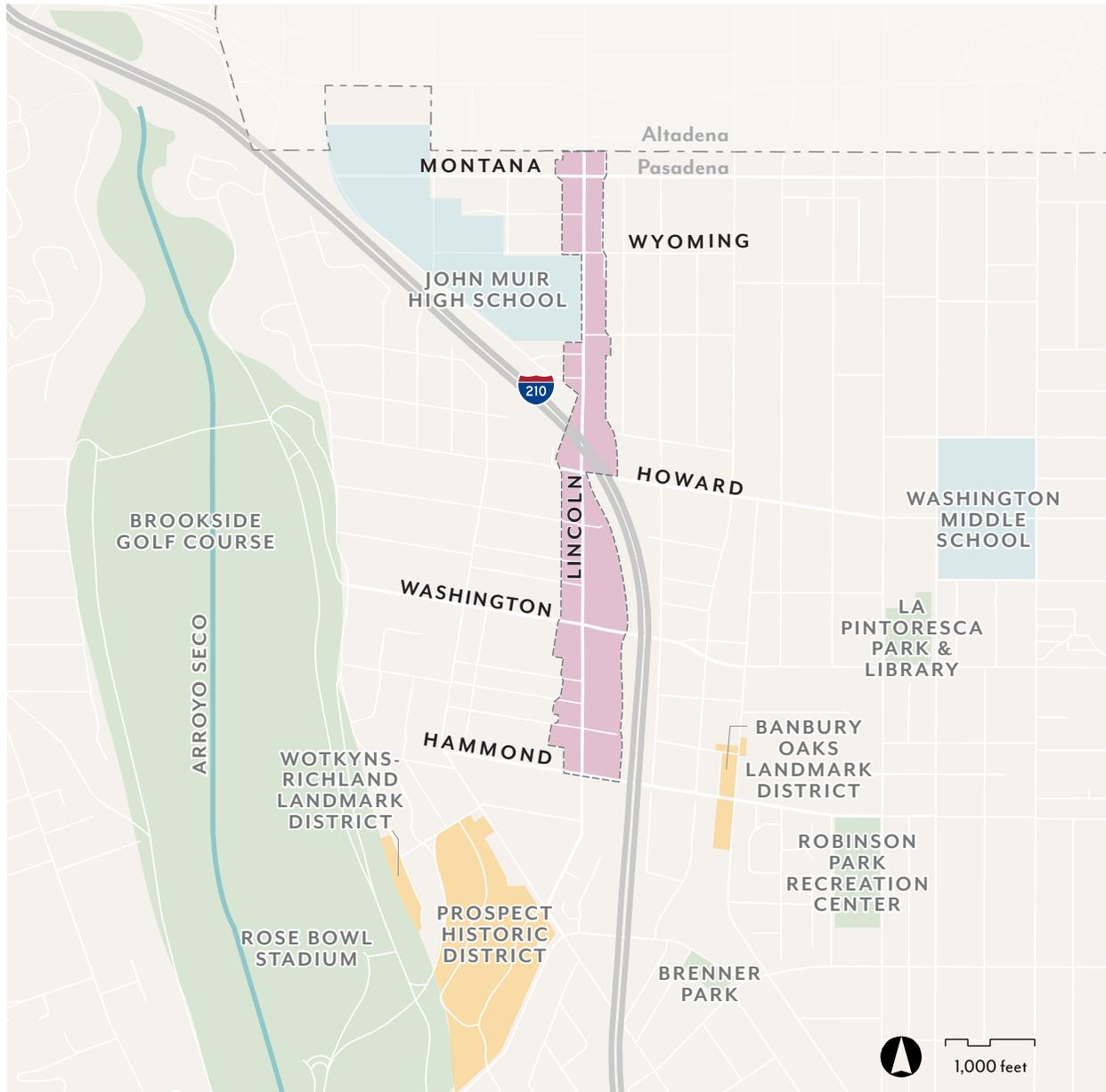
Background and Context

2.1 Community and Historic Context 17

2.2 Existing Land Uses, Urban Form,
and Public Realm 20



Map 2.1-1: Lincoln Avenue Specific Plan Area and Regional Context



Lincoln Avenue Specific Plan area

East-west streets south of the I-210 crossing provide convenient access between the Arroyo Seco and Lincoln Avenue. John Muir High School faces Lincoln Avenue abutting the Plan area, and serves as an important historic institution and center of activity in Pasadena. The I-210 bisects the Plan area just north of Howard St., connecting to La Cañada Flintridge to the northwest, and Central Pasadena to the south.

2.1 Community and Historic Context

Lincoln Avenue is a mixed-use corridor running north/south through north Pasadena, a half-mile east of the Arroyo Seco. The street is bisected by the I-210 Freeway, and surrounded by low density, primarily single-family housing. While originally a residential neighborhood serving the early subdivisions of Pasadena, many residential and commercial parcels transitioned to industrial use throughout the 20th Century, due to a stifled economic recovery from the Great Depression and permissive industrial zoning policies.

Much of the Lincoln Avenue corridor and surrounding residential neighborhood was included in one of Pasadena's few historically "redlined" communities, meaning residents were systematically blocked from mortgage or home improvement loans between 1939 and 1968 due to the Home Owners' Loan Corporation (HOLC) investment risk-grading documents. The explicitly racist and discriminatory HOLC documents, which referred to the Lincoln neighborhood as a "blighted" area with "subversive racial elements," are now infamous for their long-term influence on housing policy and access to services including banking, insurance, and healthcare within racially and economically marginalized communities throughout the United States. The construction of the I-210 in 1966 contributed further to the area's fragmentation and isolation from the City's economic center.

After decades of permissive zoning, a growing number of industrial uses such as the cement plant and auto repair shops detracted from the residential character of the area. In an effort to prevent the development of additional industrial uses and their associated impacts on the surrounding community, a group of neighborhood residents appealed to the Pasadena City Council to address these issues.

As a result, and after extensive community planning and outreach, Pasadena revised the zoning along Lincoln Avenue from Industrial to General Commercial to reduce industrial conflicts with surrounding neighborhoods and limit industrial activity. The Lincoln Avenue Redevelopment Project Area, adopted in 1986, contributed to the development of several office buildings and townhome complexes throughout the late 1980s and 1990s, some of which successfully replaced heavy industrial uses. However, as redevelopment projects were scattered throughout the area and lacked neighborhood-serving uses, they did not lead to a cohesive revitalization of the corridor. As development slowed and industrial uses continued to operate throughout the 2000s, the City identified Lincoln Avenue as the location for an eighth Specific Plan area.

In 2013, City Council approved the first Lincoln Avenue Specific Plan as a tool to catalyze a shift in land use patterns and guide development along the Lincoln Avenue corridor. The 2013 plan focused on repurposing the industrial and limited commercial corridor into a vibrant district offering more retail storefronts and neighborhood services, and increasing opportunities for housing and mixed-use development. In addition to new zoning designations and development standards, the plan included a revitalization strategy that focused on identifying and facilitating development of specific catalytic sites for growth. The 2015 General Plan reaffirmed the 2013 Specific Plan's vision for Lincoln Avenue to develop as a neighborhood-oriented district, and established Land Use designations consistent with the Specific Plan's zoning districts.



Muir High School ca. 1930; Source: Water & Power Associates

LINCOLN AVENUE TIMELINE

1900-1940

- » Early residential homes are constructed along and surrounding Lincoln Ave.
- » Early commercial development at the intersection of Lincoln Ave. and Montana St.
- » John Muir High School is constructed in 1926
- » Early development of industrial properties along Lincoln Ave.



Early residential property at 1826 Lincoln Ave.



John Muir High School



Dental Center building at 2030 Lincoln Ave.



Futaba Building at 1507 Lincoln Ave

1940-1970

- » Due to Redlining (1939-1968), the Lincoln Avenue community serves as one of Pasadena's only neighborhoods where Black, Latino, and Asian-American families could find housing throughout these decades
- » Further development of primarily commercial and industrial uses along the corridor
- » Kettle's Nursery building is constructed in 1948 as Foster's Old Fashioned Freeze ice cream shop
- » I-210 is constructed in 1966



Kettle's Nursery building at 1960 Lincoln Ave.



I-210 Freeway over Lincoln Ave. at Howard St.



Pasadena Lumber and Supplies property

LINCOLN AVENUE TIMELINE

1970-2000

- » Additional auto-oriented industrial uses are built throughout the corridor in early 1980s
- » Industrial areas are rezoned to commercial in 1985
- » Lincoln Avenue Redevelopment Project Area is adopted in 1986, leading to scattered development of office and residential uses between 1988 and 2000



Bakery at Lincoln Ave. and Del Monte St.



Public Storage between Claremont St. and Washington Blvd.



Business Park of Pasadena

2000-present

- » Slow pace of development, with some commercial space and residential townhomes added
- » First Lincoln Avenue Specific Plan is adopted in 2013
- » Our Pasadena Specific Plan Update process begins in 2018



Retail use in adaptively re-purposed building at Lincoln Ave. and Toolen St.



Multifamily residential on Lincoln Ave. between Idaho St. and the I-210 Freeway



Palm Plaza at Lincoln Ave. and Montana St.

2.2 Existing Land Uses, Urban Form, and Public Realm

The Specific Plan is organized into the following two Subareas distinguished by their existing conditions, General Plan Land Use designations, and the Plan's vision for the future:

- » Lincoln North
- » Lincoln South

This section describes existing land uses, urban form, and public realm conditions by Subareas, shown in Map 2.2-1. In Chapter 3, future-oriented goals and policies are introduced for each Subarea.

LAND USE

Land use is a characterization of how a property or building is used and describes the general activity occurring on a site, such as commercial retail, office, residential, industrial, or open space. Land uses influence the surrounding environment in a variety of ways; for example, some uses, like retail stores and restaurants, may draw pedestrians to an area and create a more active sidewalk environment, while other uses, like industrial, are generally more auto-oriented in nature.

URBAN FORM

Urban form refers to the physical form of a building, both individually and collectively within a district, and its placement within a specific site. Elements of urban form such as a building's scale and height help to determine the overall character of an area. Urban form is influenced by a combination of planning regulations and development standards, architectural design, and site-specific factors such as lot size.

PUBLIC REALM

The public realm refers to spaces that are publicly owned and/or publicly accessible. This Specific Plan regulates the portion of the public realm between private development and the roadway, typically comprised of sidewalks, parkways, street trees, and other amenities such as seating, bicycle parking, bus shelters, and trash receptacles. Other portions of the public realm such as the roadway are designed, regulated, and maintained by various other City departments and planning documents.

Map 2.2-1: Lincoln Avenue Specific Plan Subareas



Section 2.2 is reorganized.

LINCOLN NORTH

The Lincoln North subarea spans from the Specific Plan boundary near Montana Avenue to the north and the I-210 Freeway to the south. Lincoln Avenue north of Wyoming Street serves as a “main street” for the community and is characterized by a mix of retail and neighborhood services, such as coffee shops, gyms, a restaurant, a health center, and a plant nursery, which provide amenities within walking distance for the surrounding residential neighborhoods. Ground floor office uses and a manufacturing business are also present on the corridor, though these uses are not contributing to the type of active pedestrian environment characteristic of a main street area.

Urban form and lot sizes are fairly consistent in the subarea. Block lengths are small, at 200 to 300 feet, which are conducive to a walkable, main street character. Older single-story buildings on similar sized lots with primarily 60-foot frontages are the dominant form, several with alley access in the rear. While the majority of lots are individually owned, several have been consolidated through redevelopment. Traditional storefronts built to the property line, building entries open to the sidewalk, small block lengths, and parking in the rear contribute to a pedestrian-oriented character. Many buildings incorporate individual and frequent storefronts, large expanses of windows that face the street, and articulation that creates visually interesting facades. However, some buildings, while pedestrian-oriented in use, have blank walls or facades facing the street that detract from the pedestrian-friendly environment. For example, Palm Plaza at the northwest corner of Lincoln and Montana acts as a community anchor with a local café, church, and health center, but tall fencing and the siting of the building away from the street detract somewhat from the existing urban form of the district. While the office and industrial uses in this area are in buildings with traditional storefronts, many include window shades or screens which block visual access and inhibit active, pedestrian-friendly frontages.

Public realm conditions in the Lincoln North subarea are inconsistent and generally do not support the main street character of the area. Ample shade is not present as existing street trees do not provide shade or have not yet matured. Existing sidewalk widths along this portion of Lincoln Avenue are generally 10 to 12 feet, which is insufficiently wide for a main street environment with higher levels of pedestrian activity and the need for street trees, bus shelters, and other amenities.

South of Wyoming Street includes a mix of single- and multi-family homes along the two blocks that extend south to the I-210 Freeway. Residences include a combination of one- and two-story homes, townhomes, and apartments



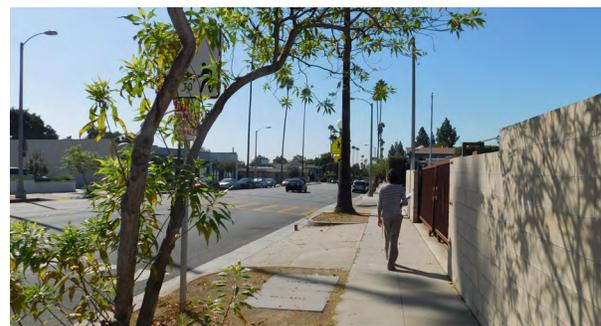
Traditional commercial uses on Lincoln Ave. across from Toolen Pl. with storefronts built to the property line



Office use in traditional storefront with inactive frontage



Palm Plaza at Lincoln Ave. and Montana St., completed in 2007



Sidewalk with insufficient tree canopy and blank walls along Lincoln Ave. at Toolen Pl.

that vary in shape and style. Lot sizes are consistent with approximately 45 feet in frontage and 140 feet in depth. Setbacks are consistently landscaped and generally between 15 to 20 feet along Lincoln Avenue. Many residences have retained their historic character, with some homes dating to the 1920s and 1930s. No properties have been designated as historic resources as of 2021 within the Specific Plan area; however, a recent historic resources survey concluded that several could be eligible for future designation. The most recent new construction, completed in 2002, is a series of townhomes in the Craftsman style at the corner of Idaho Street built on a Pasadena Community Development Commission redevelopment site.

Existing sidewalk widths in this portion of Lincoln Avenue are generally 12 to 15 feet. Sidewalks include consistent street trees and continuous parkways; however, the existing street trees provide minimal shade apart from periodic oak trees planted north of Idaho Street. This area abuts John Muir High School which serves as a significant community anchor and contributes to the public realm of the corridor with a landscaped front lawn and large shade trees. The presence of the high school has enabled east/west pedestrian crossings along this portion of Lincoln Avenue, including signal protection and high-visibility paint treatments to improve safety. Within the residential area south of the I-210 Freeway, existing sidewalk widths are 12 feet with consistent parkway trees and landscaping; however, shade from existing street trees is minimal.

The I-210 Freeway bisects the Specific Plan area contributing to the lack of a cohesive identity for the corridor. The freeway underpass' poor lighting and lack of buffer between the sidewalk and roadway inhibits pedestrian comfort and safety, discouraging travel by foot between the areas north and south of the freeway.



John Muir High School is a significant community anchor and contributes to the public realm



Townhomes at Lincoln and Idaho, constructed in 2002



Sidewalk with landscaped parkway and bus stop with bench along Lincoln Ave. at Idaho St.



I-210 Freeway overpass creates a barrier for pedestrians between the areas north and south of the freeway

LINCOLN SOUTH

The Lincoln South Subarea spans from the I-210 Freeway in the north to Hammond Street in the south. This portion of Lincoln Avenue is characterized by an eclectic combination of commercial, industrial, and institutional land uses, with a cluster of single- and multi-family residential along the west side of the corridor south of Pepper Street. Commercial and industrial uses include auto repair, construction supply and fabrication, cement batching, public storage, retail, office, a bakery, and a motel. The Business Park of Pasadena at Lincoln Avenue and Washington Boulevard is largely comprised of incubator businesses, including a robotics research and development company that benefits from close proximity to the NASA Jet Propulsion Laboratory a few miles to the north. Institutional uses consist of four churches, which are scattered throughout the Lincoln South subarea.

Urban form and lot sizes vary significantly within the subarea, though the scale is consistently one-story on both sides of Lincoln Avenue. On the west side of the street, the block lengths reflect the single-family neighborhood to the west and are generally walkable lengths between 200 to 400 feet. However, the street grid ends at Lincoln Avenue due to the I-210 Freeway to the east; therefore, the east side of the street consists of long blocks between roughly 400 and 1,300 feet in length, creating a more monotonous pedestrian environment.

On the east side of Lincoln Avenue, many lots are large, approximately 2 acres in size, reflecting uses like the business park, cement plant, public storage, and churches. On the west side of the street, though some lots have consolidated, most tend to be smaller with approximately 60-foot frontages and shallow depths of approximately 150 feet without alley access, which can make redevelopment more challenging.

The urban form is not currently pedestrian-oriented due to uneven building setbacks, frequent breaks in the street wall, and a prevalence of blank facades or fences. While several buildings are built at or near the sidewalk, many buildings are setback with parking lots at the side or front of the building, creating an auto-oriented character. Several of the sites are underdeveloped or underutilized. In the residential area in the southwest corner of the subarea, the one-story single and multifamily homes are consistently setback from the street with large front yards of at least 20 feet.



Industrial Use along Lincoln Ave. near Washington Blvd.



Church with large surface parking lot along Lincoln Ave. at Claremont St.



Public Storage with blank wall and lack of tree canopy along Lincoln Ave. at Claremont St.

Public realm conditions in the Lincoln South subarea are inconsistent and generally do not support a comfortable pedestrian experience or provide a sense of place for the community. Existing sidewalk widths are approximately 12 feet and include street trees and parkways; however, existing trees do not have large canopies or have not yet matured, creating limited shade for pedestrians and lack of a cohesive streetscape appearance. Parkway are mostly turf and inconsistent; therefore, do not significantly contribute to stormwater capture or greening of the corridor. The most consistent and well-maintained parkway condition is located along the frontage of the Business Park of Pasadena. East-west pedestrian crossings are infrequent, and most lack signal protection or other features like bulb-outs, which make crossing the street easier and safer. The lack of crossing opportunities can negatively impact the economic and physical cohesion of the subarea.

Despite these challenges, improvements have been made to the Lincoln Avenue streetscape over time including the implementation of the Department of Public Works' Master Street Tree Plan and infrastructure improvements such as undergrounding of utilities, installation of uniform street furniture that includes bus benches and trash receptacles, and pedestrian-scale lighting. Continued investments can help to unify Lincoln North and South through a high-quality public realm that includes consistent street trees and amenities, as well as draw together various land uses through a consistent urban form and pedestrian experience. These opportunities are addressed in this Specific Plan within Chapter 5 (Public Realm Standards and Design Guidelines) and Chapter 7 (Implementation and Administration), as well as Appendix A.2 (Design Guidance for Tree Selection).



Multi-family homes along Lincoln Ave. at Pepper St. feature large front yard setbacks



Sidewalk with limited street tree canopy along Lincoln Ave. at Del Monte St.



Sidewalk with landscaped parkway at the Business Park of Pasadena along Lincoln Ave. at Washington Blvd.

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Ch. 3

Vision, Goals & Policies

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Vision, Goals & Policies

CHAPTER OVERVIEW

The Lincoln Avenue Specific Plan Vision, Goals, and Policies establish the desired outcomes of the Plan and provide general direction for achieving these outcomes.

VISION

- » The vision characterizes the intended future of the Lincoln Avenue corridor, as shaped by both the General Plan and extensive community input during the Plan update process. This Plan's Vision contains an overarching vision statement and six supporting vision objectives.

GOALS

- » A goal is a statement that describes a desired future condition or “end” state. Goals are change and outcome oriented, achievable over time, though not driven by funding.

POLICIES

- » A policy is a clear statement that guides a specific course of action for decision-makers to achieve the associated goal.

The vision, goals, and policies in this chapter are presented in the following sections:

- » **3.1 Vision.**

- » 3.1.1 Vision Statement
- » 3.1.2 Vision Objectives

- » **3.2 Goals & Policies.**

- » 3.2.1 Plan Area Goals & Policies
- » 3.2.2 Subarea Goals & Policies

3.1 Vision

Section 3.1 and 3.2 are reorganized.

3.1.1 VISION STATEMENT

Lincoln Avenue will be the heart of the greater residential neighborhood where commercial and multi-family uses co-exist in a walkable, vibrant environment with public realm improvements that create a cohesive and well-connected corridor.

3.1.2 VISION OBJECTIVES

1. Neighborhood Character

Compatible development that adds to the rich cultural history of the community

4. Connected Neighborhood

Enhanced sidewalks that support pedestrian comfort and safety, and create a cohesive sense of place

2. Commercial Core

A strong spine of retail and services north of Wyoming Street, supported by reuse of existing buildings and targeted infill development

5. Greening & Open Space

A more livable and sustainable community with rich landscaping and open space

3. Housing Choices

A variety of new housing options that complement existing neighborhoods

6. Innovation Hub

Clean industrial and commercial uses that provide future-friendly jobs

3.2 Goals & Policies

The Goals and Policies in this section provide policy direction for implementing the Plan's vision and achieving the desired outcomes based on community input and General Plan guidance. Goals and policies also provide guidance to decision makers such as City staff, City Commissions, or City Council when reviewing development projects, and they can also help support grant funding efforts to supplement the City budget for public improvement projects.

The Plan includes goals and policies that are applicable to specific subareas, as well as the entire plan area. Goals and policies for the entire plan area are organized by topic:

- » Public realm & Community Cohesion
- » Development & Design
- » Economic Development
- » Multi-family Housing



Shaded outdoor seating and street trees enhance storefronts and contribute to neighborhood identity.

PUBLIC REALM & COMMUNITY COHESION

Goal 1. Sidewalks and other spaces that are accessible to the general public, and contribute to Lincoln Avenue’s overall identity and sense of place.

Policies:

- 1.a. Walkability.** Provide an unobstructed path of travel for users of all abilities that can reasonably accommodate pedestrian volumes along the corridor.
- 1.b. Public Amenities.** Provide a designated portion of the sidewalk adjacent to the street right-of-way for public amenities such as seating, bicycle parking, trash receptacles, bus shelters, and parkways, tree wells, or other forms of landscaping.
- 1.c. Enhanced Storefronts.** If amenities and walk space are provided by property owners, allow a designated area adjacent to private property that allows for door openings from buildings, sidewalk seating, planters, and other enhancements to a building’s entrance and ground floor exterior.

Goal 2. A comfortable and well-connected corridor that encourages sustainable modes of travel such as walking, biking, rolling, and bus riding.

Policies:

- 2.a. Multi-Modal Environment.** Encourage non-driving modes of travel by providing sufficient space for installations including bus shelters and bicycle racks.
- 2.b. I-210 Freeway Underpass.** Improve visual and physical conditions under the I-210 Freeway to better connect the north and south of the Plan area and promote walking within the district.
- 2.c. East/West Mobility.** Support the addition of signalized crossings across Lincoln Avenue, as well as bulb-outs, crosswalk treatments, landscaped medians, lighting, and other improvements that promote safe, comfortable east-west connections.
- 2.d. Bicycle Connections.** Support a more bicycle-friendly corridor by enhancing safety within the current shared roadway, providing signage to nearby bicycle facilities, and exploring the potential for enhanced bicycle infrastructure along Lincoln Avenue.



Landscaping and street trees in the public realm help to enhance storefronts and contribute to neighborhood identity.



Seating, bicycle parking, trash and recycling receptacles, and shade trees provide a comfortable and amenity-rich pedestrian experience

PUBLIC REALM & COMMUNITY COHESION

Goal 3. A safe, socially and culturally connected corridor supported by public art and community events.

Policies:

- 3.a. Activated Open Space.** Collaborate with local institutions and property owners to activate open spaces such as the John Muir High School lawn for community events.
- 3.b. Public Art.** Encourage collaboration between City departments and the Public Art Commission, Northwest Commission, and local businesses to identify locations for public art installations and other aesthetic improvements that reflect and build upon the Lincoln Avenue community identity.
- 3.c. Wayfinding and Signage.** Incorporate signage that helps to build a sense of place and community while also providing direction to nearby places of interest.

Goal 4. A green corridor with sufficient landscaping and shade coverage to encourage pedestrian mobility and support sustainability objectives such as carbon sequestration, mitigating the urban heat island effect and enhancing stormwater capture.

Policies:

- 4.a. Parkways.** Incorporate parkways into the public sidewalk, providing opportunities for street tree planting, improving permeability for rain and stormwater capture, and cooling the sidewalk environment.
- 4.b. Street Tree Distribution.** Increase the frequency and consistency of canopy trees to improve air quality and allow pedestrians to walk the length of the corridor in a shaded environment.
- 4.c. Street Trees.** Encourage street tree planting that supports the aesthetic objectives of local commercial businesses within a walkable retail- and services-oriented environment, and that have tree canopies appropriate to the scale and setbacks along the corridor.
- 4.d. Landscaped Setbacks.** Incorporate thoughtful landscaping with sustainable and native plant materials in areas where wider, buffered setbacks are appropriate.

DEVELOPMENT & DESIGN

Goal 5. Complementary building forms that fit the scale of the neighborhood and support a vibrant, walkable district.

Policies:

- 5.a. Architectural Diversity.** Allow for a range of architectural styles and forms that provide visual interest and quality design through massing and façade standards.
- 5.b. Scaled Transitions.** Provide appropriate upper floor setbacks where new development may be adjacent lower density districts such as single-family residential.
- 5.c. Transparency.** Require facade transparency, particularly on the ground floor, that improves architectural design and provides visual interest to pedestrians.
- 5.d. Blank Walls.** Reduce the prevalence of blank walls and facades along the corridor.

Goal 6. Ample access to open space for both passive and active enjoyment.

Policies:

- 6.a. Residential Open Space.** Incorporate private and common open space areas that correlate to a building's size and number of residents.
- 6.b. Commercial Open Space.** Require large non-residential or mixed-use projects to provide open space for employees, visitors, and the public.
- 6.c. Quality Design.** Introduce open space design standards meant to create usable and functional open space for residents, employees, and visitors alike.
- 6.d. Urban Greening.** Use open space areas to further environmental goals – such as carbon sequestration and reducing the urban heat island effect – through tree planting, stormwater capture, and native landscaping.

ECONOMIC DEVELOPMENT

Goal 7. A supportive environment for new development and businesses that are compatible with surrounding residential uses.

Policies:

- 7.a. Small Business Support.** Leverage the Northwest Programs and Economic Development and Planning Offices to assist with new business siting, facilitate permitting, provide training for entrepreneurs, and coordinate with City departments.
- 7.b. Balanced Requirements.** Elevate quality of design while maintaining a reasonable level of flexibility to increase market feasibility for new developments.
- 7.c. Lower Barriers to Entry.** Exempt small businesses from costly parking requirements and simplify parking requirements among different uses, to make it easier to fill vacant spaces with new tenants.
- 7.d. Arroyo Seco Activity.** Leverage proximity to events at the Arroyo Seco and Rose Bowl to create economic benefits for local businesses.
- 7.e. Unbundled Parking.** Separate the cost of parking from the costs of housing to ensure that non-car owners do not pay for parking they do not need.



Highlight Coffee, a new small business in the Lincoln North subarea

MULTI-FAMILY HOUSING

Goal 8. Attractive, welcoming multi-family districts that positively contribute to the neighborhood’s sense of place.

Policies:

- 8.a. Reinvestment.** Encourage investment in maintenance of existing properties and construction of new buildings with similar forms.
- 8.b. Range of Opportunity.** Allow for a variety of low-to-medium densities, from duplex to townhomes to apartments at various price points.
- 8.c. Courtyard-Style Housing.** Continue to encourage courtyard-style building forms across densities and housing types, providing centralized open space for residents.
- 8.d. Large Setbacks.** Setbacks that are consistent with single-family homes and older multi-family buildings within the district.
- 8.e. Greening.** Contribute to overall greening along the corridor through large front yards and well-maintained parkways within the sidewalk.



New multi-family housing at the corner of Lincoln Ave. and Idaho St.

3.2.2 SUBAREA GOALS & POLICIES

LINCOLN NORTH SUBAREA

Goal 9. A small-scale, neighborhood-serving commercial main street with a vibrant pedestrian atmosphere and historic charm, within walking distance of courtyard style multi-family housing and John Muir High School.

Policies:

- 9.a. Neighborhood Core.** Foster a two-block commercial main street between Wyoming and Montana Streets with diverse commercial uses that will serve as a focal point for surrounding neighborhoods.
- 9.b. Lincoln/Wyoming Intersection.** Establish the Lincoln Avenue and Wyoming Street intersection as a prominent gateway to the neighborhood commercial core with public open space opportunities at John Muir High School and the Kettle's Nursery property.
- 9.c. Adaptive Reuse.** Encourage sensitive reuse of existing buildings to achieve the preservation and rehabilitation of both designated and undesignated historic properties.
- 9.d. Multi-family Housing.** Support multi-family housing that reflects the neighborhood's historic residential and commercial character, and complements John Muir High School.
- 9.e. Proximity to Services.** Allow for neighborhood businesses that provide everyday shops and services within walking distance for residents.
- 9.f. Ground Floor Uses.** Balance the desire for active ground floor uses with changing market demand by allowing use flexibility while restricting uses that do not meet the overall vision for the subarea.
- 9.g. Ground Floor Design.** Incorporate design standards and guidelines relating to entrances, transparency, and other elements that help activate the ground floor and create a connection between the public and private realms.
- 9.h. Historic Resources.** Preserve architecturally and culturally significant structures, and encourage future alterations or repairs to maintain the resource's character-defining features.
- 9.i. Setbacks.** Minimize building setbacks to reinforce the historic streetwall, and use any setback area to provide additional sidewalk, amenities or aesthetic enhancements to activate the public realm.
- 9.j. Shade.** Ensure that areas adjacent to local businesses are comfortably shaded through the provision of awnings, street trees, and/or shade umbrellas as appropriate.
- 9.k. Kettle's Nursery Building.** Create site-specific guidelines for the appropriate rehabilitation of the building that acknowledges its importance and celebrates its history.
- 9.l. Freeway Adjacent Parcels.** Limit development of PS zoned sites and new parcels abutting the I-210 freeway to non-residential uses consistent with the LASP-CL Zoning District.

LINCOLN SOUTH SUBAREA

Goal 10. A livable employment hub with new multi-family housing options and flexible commercial spaces for research and development and other new businesses, coupled with shopping, amenities, and services within walking distance of residents and employees.

Policies:

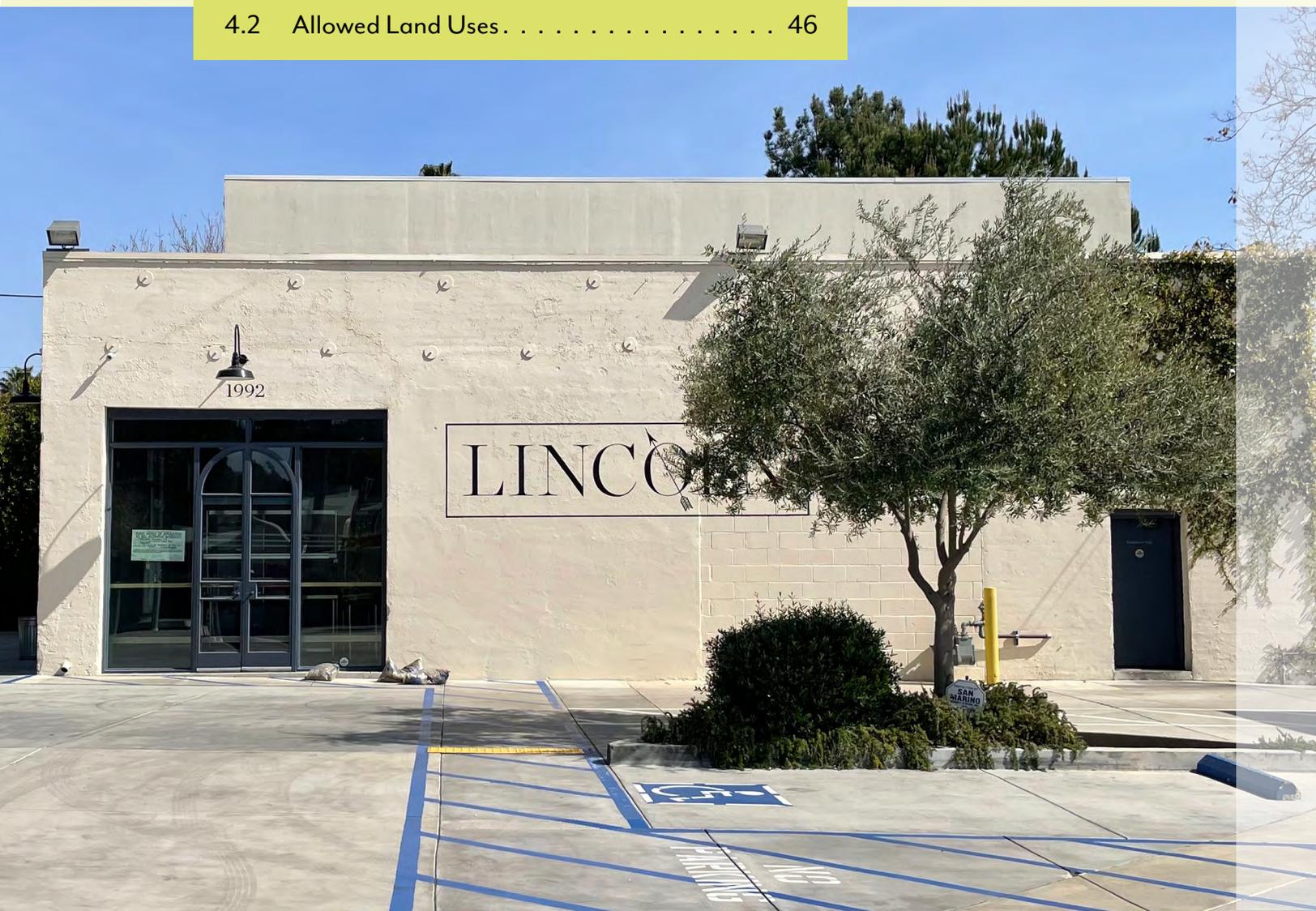
- 10.a. Targeted Growth.** Focus new housing construction in low intensity, commercial infill areas, helping to address the City's housing needs while protecting existing housing and replacing incompatible uses.
- 10.b. New Amenities.** Allow for commercial uses on the ground floor to encourage new amenities within walking distance of local residents.
- 10.c. Mix of Uses.** Allow full flexibility for properties on the west side of Lincoln Avenue north of Pepper Street to develop as a mixed-use, residential, and/or commercial project, but limit upper floor commercial to maintain residential character for nearby neighbors.
- 10.d. Multi-family Housing.** Support multi-family housing on the west side of Lincoln Avenue in close proximity to jobs and amenities within the adjacent commercial flex and mixed-use areas.
- 10.e. Lincoln / Washington Intersection.** Establish the Lincoln Avenue and Washington Boulevard intersection as a key community node with public open space, pedestrian improvements, accent street trees, landscaping, and complementary new development.
- 10.f. Incompatible Uses.** Phase out heavy industrial and auto repair uses that are incompatible with nearby residential uses.
- 10.g. Flexible Spaces.** Replace old industrial uses on the east side of Lincoln Avenue with new buildings that can accommodate a wide range of established industries, start-up incubators, and creative businesses as part of Pasadena's broader economic strategy.
- 10.h. Freeway Buffer.** Allow commercial uses on the east side of Lincoln Avenue to act as a buffer between the I-210 freeway and the residential uses to the west.

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Ch. 4

Zoning and Land Use

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SILVERLAKE



Zoning and Land Use

CHAPTER OVERVIEW

The zoning and land use regulations in this chapter are intended to guide development and decision making to achieve the vision of the Lincoln Avenue Specific Plan.

While broad land use categories are assigned in the General Plan, the Specific Plan establishes a detailed list of allowed land uses and permit requirements for each zoning district within the plan area.

This chapter is organized into the following sections:

- » **4.1 Zoning Districts.**
- » **4.2 Allowed Land Uses.**



4.1 Zoning Districts

4.1.1 PURPOSE

The purpose of the Lincoln Avenue Specific Plan zoning districts is to implement the Plan vision for each of the districts, described below.

LASP-CG

Commercial General

- » Foster a pedestrian-oriented neighborhood core
- » Accommodate a diverse range of retail and office businesses that people can walk to for shopping, dining, personal and community services, and social activities

LASP-MU

Mixed-Use

- » Allow pedestrian-oriented ground floor commercial uses
- » Support projects that are entirely commercial, entirely residential, or a mix of the two, integrated either horizontally or vertically

LASP-CL

Commercial Limited

- » Allow existing commercial uses to remain in place
- » Ensure that future uses are compatible with neighboring homes

LASP-RM-16

Residential Multi-family

- » Allow a variety of multi-family and compatible residential uses of an appropriate scale

LASP-CF

Commercial Flex

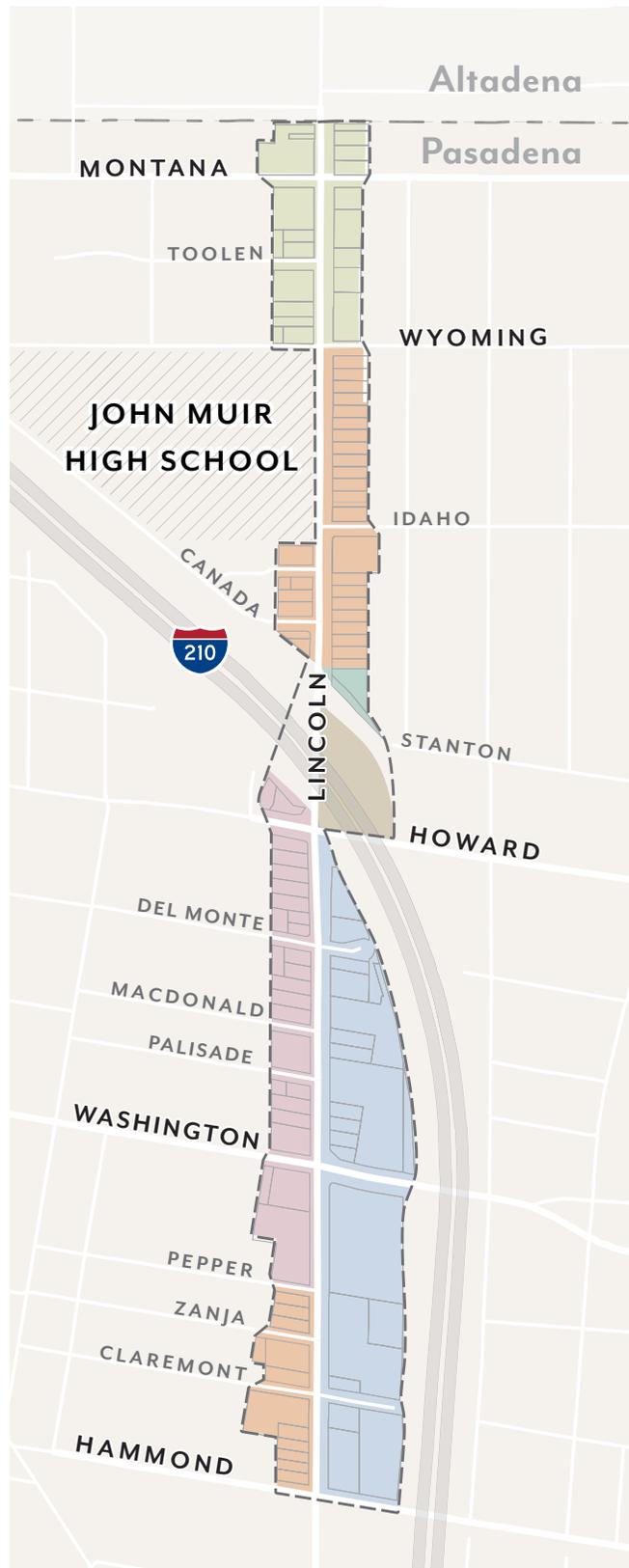
- » Allow a wide range of commercial and research & development uses
- » Provide flexibility for the city's burgeoning innovation and production industries
- » Restrict industrial uses that are inappropriate next to residential uses

PS

Public-Semipublic

- » Provide for institutional uses that may not be appropriate in other base zoning districts

Map 4.1-1: Zoning Districts



4.1.2 APPLICABILITY

The standards of this Specific Plan apply to proposed development and new land uses in all zones except PS. In PS, development shall be subject to a Conditional Use Permit or Master Plan per Pasadena Municipal Code (PMC) 17.26.

In LASP-RM-16, development shall follow all standards of RM-16 zoning in PMC 17.22 unless modified by this Specific Plan.

- LASP-CG
- LASP-CL
- LASP-CF
- LASP-MU
- LASP-RM-16
- PS

4.2 Allowed Land Uses

4.2.1 LAND USES AND PERMIT REQUIREMENTS

- A. **Permit Requirements.** Table 4.2-1 identifies the uses of land allowed by this Specific Plan, the land use permit required to establish each use, and limitations that may apply for a particular use.
1. Definitions of specific land uses are found in PMC 17.80.020.
 2. Additional standards for specific land uses may apply; refer to the PMC Section noted in the table.
- B. **Upper Floors.** In MU-48, stories above the ground floor are limited to residential uses; nonresidential uses are prohibited.
- C. **Alcohol Sales.** The sale of alcohol is conditionally permitted only as an accessory to the following uses where permitted.
- » On-site consumption: A restaurant or alcohol beverage manufacturing (i.e. brewery, distillery tasting room).
 - » Off-site consumption: Retail food sales in commercial spaces \geq 15,000 square feet; floor space for alcohol shall be no more than 5% of the store's total floor area, including both sales and storage.
- D. **Prohibited Uses.** Those uses not listed in Table 4.2-1 are prohibited by this Specific Plan, except as otherwise provided by PMC17.21.030.A.
1. Drive-throughs associated with any use are prohibited.
- E. **Nonconforming Uses.** Existing uses which are made nonconforming by this Specific Plan shall not be expanded and are further subject to the provisions of PMC17.71.

Table 4.2-1: Allowed Uses, Permit Requirements & Specific Limitations

Symbol	Description	PMC Section
P	Permitted use, Code Compliance Certificate required.	17.61.020
MC	Conditional use, Minor Conditional Use Permit required.	17.61.050
C	Conditional use, Conditional Use Permit required.	
E	Conditional use, Expressive Use Permit required.	17.61.060
TUP	Temporary use, Temporary Use Permit required.	17.61.040
—	Use not allowed.	

ZONING DISTRICT LAND USES AND PERMIT REQUIREMENT						
Land Use ¹	Permit Requirement					PMC Section / Notes
	LASP-CG	LASP-CL	LASP-CF	LASP-MU	LASP-RM-16	
RESIDENTIAL USES						
Accessory Dwelling Unit	—	—	—	P	P	17.50.275
Home Occupations	—	—	—	P	P	17.50.110
Multi-family Residential	—	—	—	P	P	
Residential Accessory Uses and Structures	—	—	—	P	P	17.50.250
Residential Care, Limited	—	—	—	P	P	
Transitional Housing	—	—	—	p ²	p ²	Maximum
RECREATION, EDUCATION & PUBLIC ASSEMBLY USES						
Clubs, Lodges, Private Meeting Halls	C	C	C	C	C	17.50.230
Colleges, Nontraditional Campus Setting	P	P	P	P	—	
Commercial Entertainment	E	—	E	E	—	17.50.130
Commercial Recreation, Indoor	P	—	P	P	—	17.50.130
Commercial Recreation, Outdoor	C	—	C	—	—	17.50.130
Cultural Institutions	P	P	P	P	C	
Electronic Game Centers	C	—	C	C	—	17.50.100
Park and Recreation Facilities	P	P	P	P	P	
Religious Facilities	C	C	C	C	C	17.50.230
with Columbarium	MC	MC	MC	MC	—	17.50.230
with Temporary Homeless Shelter	C	C	C	C	—	17.50.230
Schools, Public and Private	C	C	C	C	C	17.50.270
Schools, Specialized Education and Training	P	P	P	P	—	17.61.050.J

¹ See PMC 17.80.020 for definition of the listed land uses, except those listed in footnotes 2 and 3.

² The maximum interior or exterior area in which support services are offered or located shall not exceed 250 square feet.

ZONING DISTRICT LAND USES AND PERMIT REQUIREMENT						
Land Use ¹	Permit Requirement					PMC Section / Notes
	LASP-CG	LASP-CL	LASP-CF	LASP-MU	LASP-RM-16	
OFFICE, PROFESSIONAL & BUSINESS SUPPORT USES						
Automated Teller Machines (ATMs)	P	P	P	P	—	17.50.060
Banks and Financial Services	P	P	P	P	P	17.61.050.J
with Walk-Up Services	P	P	P	P	—	17.50.060
Business Support Services	P	P	P	P	—	17.61.050.J
Offices, Accessory	P	P	P	P	—	17.61.050.J
Offices, Administrative Business Professional	P	P	P	P	—	
Offices, Government	P	P	P	P	—	
Offices, Medical	P	P	P	P	—	
Offices, Research and Development	P	P	P	P	—	17.50.240, 17.61.050.J
Work/Live Units	—	—	P	P	—	17.50.370
RETAIL SALES						
Accessory Tasting Rooms ³	—	—	C	—	—	Refer to Section 4.2.1.C, 17.50.040
Alcohol Sales, Beer and Wine	C	C	C	C	—	
Alcohol Sales, Full Alcohol	C	C	C	C	—	
Animal Retail Sales	P	—	—	—	—	
Commercial Nurseries	C	C	C	C	—	17.50.180
Convenience Stores	C	C	C	C	—	
Food Sales	P	—	P	P	—	
Restaurants, Fast Food	P	—	P	P	—	17.50.260
Restaurants, Formula Fast Food	P	—	P	P	—	17.50.260
Restaurants	P	P	P	P	—	17.50.260, 17.61.050.J
with Limited Live Entertainment	P	—	P	P	—	
with Walk-Up Window	C	C	C	C	—	
Retail Sales	P	P	P	P	—	Retail stores may not exceed 40,000 square feet in size.
Service Stations	C	—	—	—	—	17.40.070; 17.61.050.J

¹ See PMC 17.80.020 for definition of the listed land uses, except those listed in footnotes.

³ **Accessory Tasting Rooms** is defined as uses accessory to an alcohol manufacturing plant that offer on-site tastings and sell beverages manufactured on the premises for on-site or off-site consumption. The subcategory includes establishments such as breweries, wineries, and distilleries that offer tastings and sales of alcohol beverages in accordance with a license issued by the California Department of Alcoholic Beverage Control.

ZONING DISTRICT LAND USES AND PERMIT REQUIREMENT						
Land Use ¹	Permit Requirement					PMC Section / Notes
	LASP-CG	LASP-CL	LASP-CF	LASP-MU	LASP-RM-16	
SERVICES						
Adult Day Care, Limited	P	P	P	P	P	
Animal Services, Grooming	P	P	P	P	—	
Catering Services	P	P	P	P	—	17.61.050.J
Charitable Institutions	P	P	P	P	—	17.61.050.J
Child Day Care Centers	C	C	C	C	C	17.50.080
Child Day Care, Large	—	P	—	P	P	17.50.080
Child Day Care, Small	—	P	—	P	P	
Laboratories	P	MC	P	MC	—	
Maintenance and Repair Services	P	—	P	P	—	
Massage Establishments	C	—	—	C	—	17.50.155, 17.61.050.J
Mortuaries/Funeral Homes	C	—	C	—	—	
Neighborhood/Community Gardens	P	P	P	P	P	
Personal Improvement Services	P	P	P	P	—	17.61.050.J
Personal Services	P	P	P	P	—	
Printing and Publishing, Limited	P	P	P	P	—	17.61.050.J
Public Safety Facilities	C	C	C	C	C	
INDUSTRY, MANUFACTURING & PROCESSING						
Alcohol Beverage Manufacturing ⁴	—	—	C	—	—	Refer to Section 4.2.1.C
Custom Manufacturing / Artisan Production ⁵	—	—	P	—	—	
Recycling Centers, Small	—	—	MC	—	—	17.50.220
Research and Development, Non-offices	C	C	P	—	—	17.50.240

¹ See PMC 17.80.020 for definition of the listed land uses, except those listed in footnotes.

⁴ **Alcohol Beverage Manufacturing** is defined as a use where manufacturing of beer, wine, or other alcohol beverages are processed or prepared for consumption.

⁵ **Custom Manufacturing / Artisan Production** is defined as an artisanal, independent, or small-scale use limited to a maximum gross floor area of 15,000 square feet that involves the assembly, compounding, design, development, evaluation, manufacturing, processing, packaging, or treatment of components into products and conducted within enclosed buildings. These uses do not produce noise and vibration beyond the property line. Uses requiring State or Federal emissions permits are excluded from this use category to protect neighboring properties. Truck trips are limited to maximum of 10 per day. Small-scale food production including commercial bakeries, but excluding catering, are included in this use category. Accessory uses that support the primary use of the establishment may comprise up to 25% of the gross floor area of the establishment. Accessory uses may include those such as, but not limited to, outdoor dining, on-site food and beverage tastings, and retail.

ZONING DISTRICT LAND USES AND PERMIT REQUIREMENT						
Land Use ¹	Permit Requirement					PMC Section / Notes
	LASP- CG	LASP- CL	LASP- CF	LASP- MU	LASP- RM-16	
TRANSPORTATION, COMMUNICATIONS, AND UTILITY USES						
Accessory Antenna Arrays	P	P	P	P	—	17.40.070
Commercial Off-Street Parking	C	C	C	—	—	
Communications Facilities	C	C	C	C	—	
Transportation Terminals	C	C	C	C	—	17.50.310
Wireless Telecom Facilities, Major	C	C	C	C	—	
Wireless Telecom Facilities, Minor	MC	MC	MC	MC	—	
Wireless Telecom Facilities, SCL	P	P	P	P	—	
Utilities, Major	C	C	C	C	C	
Utilities, Minor	P	P	P	P	P	
TEMPORARY USES						
Filming, Long-term	C	C	C	C	C	
Filming, Short-term	P	P	P	P	P	
Street Fairs	P	P	P	P	P	
Tents	TUP	TUP	TUP	TUP	TUP	17.50.320
Personal Property Sales	—	—	—	P	P	17.50.190
Seasonal Merchandise Sales	P	P	P	P	—	17.50.180; 17.61.050.J
Other Temporary Uses	TUP	TUP	TUP	TUP	TUP	

¹ See PMC 17.80.020 for definition of the listed land uses, except those listed in footnotes.

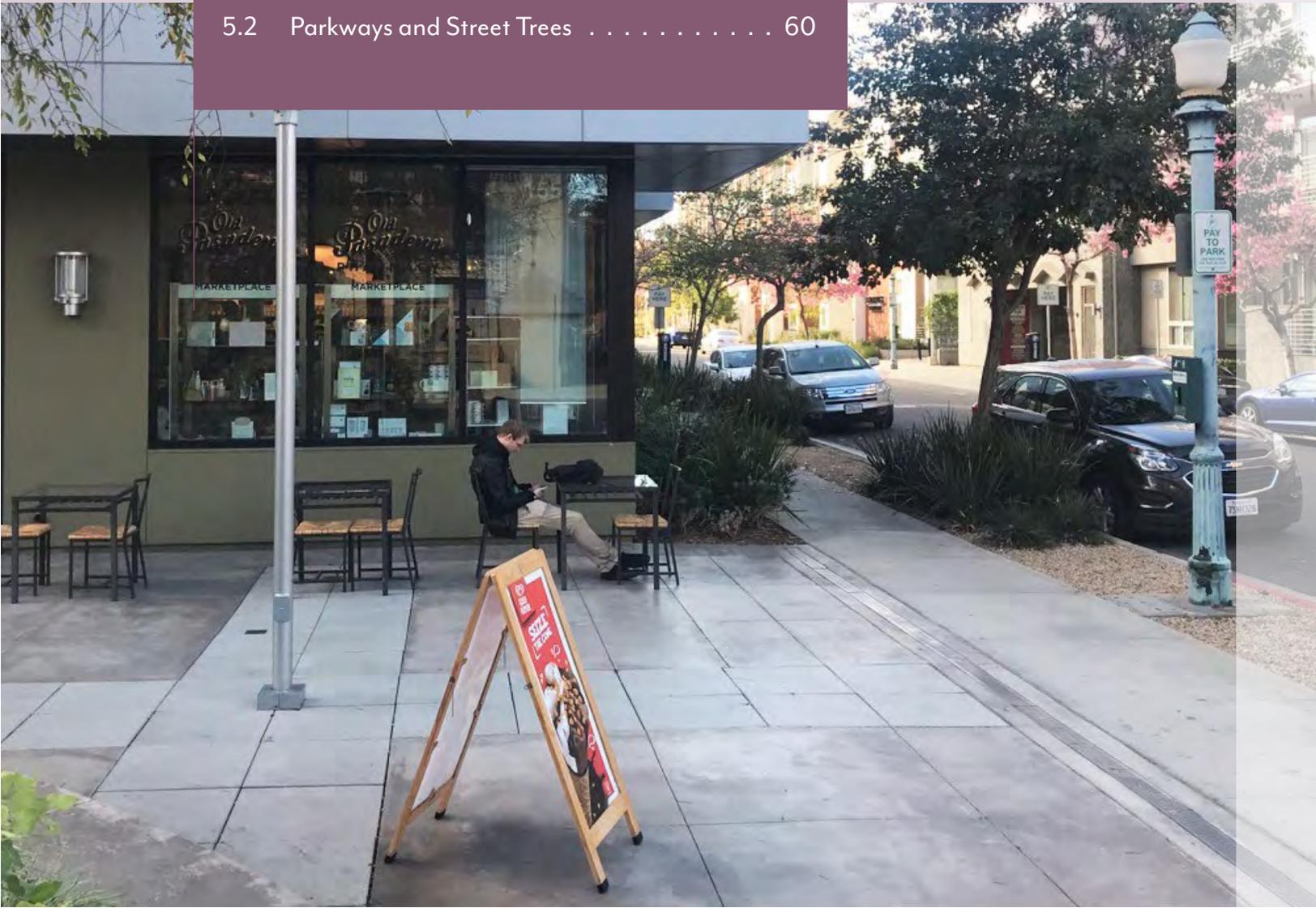
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Ch. 5

Public Realm

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Public Realm

CHAPTER OVERVIEW

The public realm standards and guidelines in this chapter serve to implement the General Plan vision for Lincoln Avenue Specific Plan area and achieve objectives of the Pasadena Street Design Guide and Pasadena Master Street Tree Plan. To improve the public realm for users of all abilities, and to provide enough space for simultaneous uses of the sidewalk, these standards and guidelines ensure that new developments contribute to the safety, accessibility, and connectivity of their surrounding streetscape network. Many features that are critical to walkability depend on the width and organization of the sidewalk. For example, consistent street trees provide shade and other aesthetic and environmental benefits, and sidewalk seating for restaurants and cafés activate the public realm and boost business. However, the success of both relies on the sidewalk offering ample and well-organized space to prevent conflicts with pedestrians.

The public realm standards and guidelines in this chapter address and regulate pedestrian infrastructure and amenities to support a safe, accessible, and comfortable pedestrian experience. The standards and guidelines are presented in the following sections:

- » **5.1 Sidewalks.** Addresses minimum sidewalk widths and sidewalk zones.
- » **5.2 Parkways and Street Trees.** Addresses parkway dimensions, amenities, and materials, and street tree placement and preferred species.

Each section includes rationale for the standard followed by sub-sections for individual standards, if applicable. Each standard is introduced in text and/or table format with diagrams and images to illustrate regulations. Supplementary text boxes are provided for additional context on most standards and diagrams. Note that diagrams are provided for the purposes of communicating measurements and images are included to illustrate potential outcomes of the standards; neither are suggestive of regulated architectural styles.



Sidewalks with sufficient width can support pedestrian travel as well as space for various amenities.



A well-designed public realm provides comfortable and accessible space for people of all abilities.

PASADENA STREET DESIGN GUIDE

Pasadena's Street Design Guide provides a framework for understanding the way sidewalks are used, and organizes sidewalks into zones to avoid conflict between various uses and amenities. Requirements vary based on the level of activity, land uses, intensities, and densities, as well as special conditions. Through designating specific zones, the Lincoln Avenue Specific Plan can help enhance the pedestrian experience by increasing sidewalk widths, enabling more shade coverage and opportunities for amenities such as seating and landscaping.

The Street Design Guide organizes sidewalks into the following three zones, which provide a basis for standards in the Specific Plan:

- » The **Amenity / Curb Zone (Amenity Zone)** is the portion of the sidewalk directly adjacent to the street right-of-way. This zone typically includes street trees, street lights, parkways, street furniture, bicycle parking, bus shelters, and other utility facilities.
- » The **Walk Zone** is the portion of the sidewalk dedicated to pedestrian travel and shall be free of obstruction.
- » The **Building Frontage Zone** is adjacent to private property and allows for door openings from buildings, outdoor furniture and shade structures.

Figure 5.1-1: Sidewalk Zones



5.1 Sidewalks

Standards in sections 5.1 shall apply to all Projects as defined in PMC 17.80.020. These standards are intended to:

- » Ensure a minimum sidewalk width is achieved, appropriate to support future densities, intensities, uses, and pedestrian volumes;
- » Provide sufficient space to support dedicated amenity and walk zones; and
- » Increase shade, carbon sequestration, and stormwater capture by allowing adequate space for street trees and parkways.

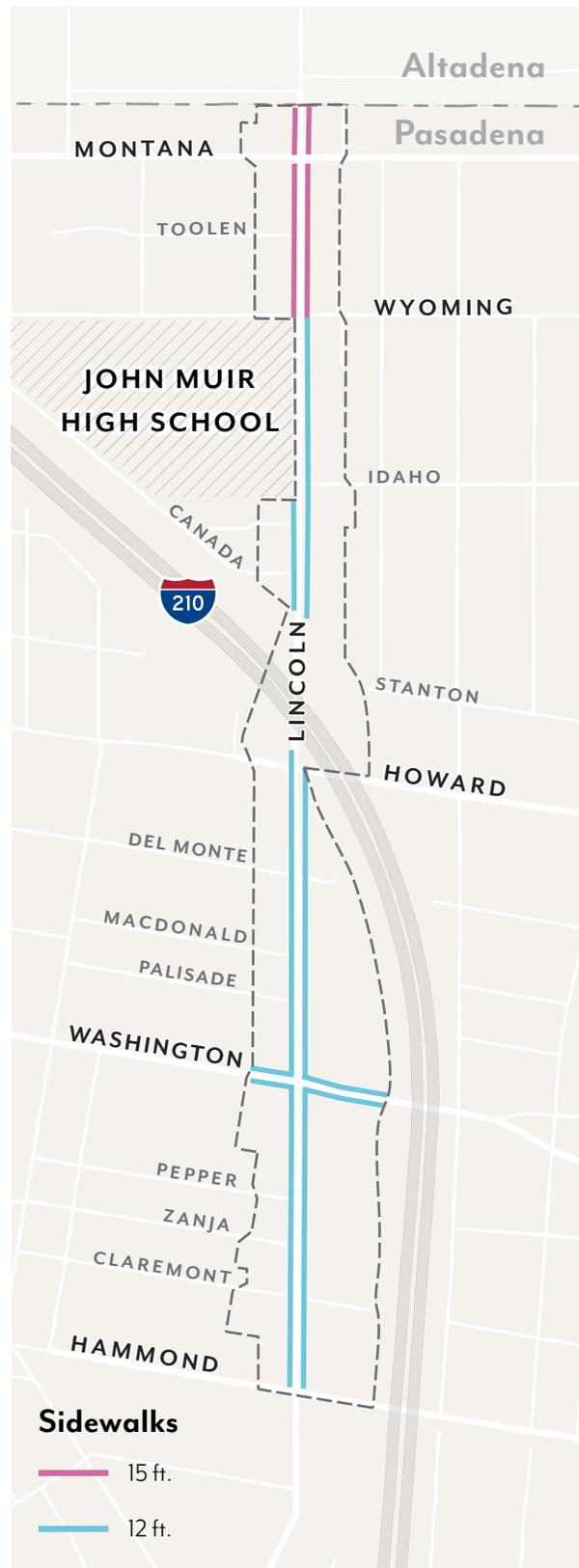
5.1.1 SIDEWALK WIDTH

- A. **Dimension.** Projects shall provide sidewalks that meet the required widths per Map 5.1-1. Where the existing sidewalk right-of-way is less than the required width, the difference shall be provided through a dedication.
1. Sidewalks are measured from the Primary Curb Line of each block to the sidewalk line, as illustrated in Figure 5.1-2.
 2. This area shall be paved for general use to the standards specified by Public Works, except for landscaped parkways per Section 5.2.
 3. Within the sidewalk width, sidewalk zones shall be provided to the dimensions set in Figures 5.1-3 through 5.1-6.
 4. Where the curb deviates (i.e. bulb-outs), exceptions in zone width are allowed and shall be determined by Public Works.
 5. Driveways are allowed per Section 6.6.1.
- B. **Maintenance.** Sidewalk improvements shall be installed and maintained by the abutting property owner(s).

SIDEWALK WIDTHS

Sidewalk widths of at least 12' are required throughout the Specific Plan area to provide space for a clear walk zone and basic amenities such as landscaping, lighting, signage, and bicycle parking. Sidewalks of 15' are required in commercial areas with more pedestrian activity and greater need for amenities.

Map 5.1-1: Sidewalk Widths



5.1.2 SIDEWALK ZONES

- A. **Amenity Zone.** Sidewalks shall provide an amenity zone at the width illustrated in Figures 5.1-3 through 5.1-6, including the curb.
1. Projects shall meet minimum parkway and street tree requirements per Section 5.2.
 2. The following elements are permitted in the Amenity Zone at the discretion of Public Works:
 - a. Paved area for pedestrian mobility,
 - b. Parkway and street trees,
 - c. Seating/furniture,
 - d. Outdoor dining (with a Public Works permit),
 - e. Planters,
 - f. Bicycle parking,
 - g. Bus shelters, and/or
 - h. Other utility facilities including streetlights, signals, meter and sign poles, and pullboxes, etc.
- B. **Walk Zone.** Sidewalks shall maintain a minimum continuous path of travel for pedestrians at the width illustrated in Figures 5.1-3 through 5.1-6. This area shall be free of all furnishings, landscaping, or obstructions.

IMPORTANCE OF SIDEWALKS

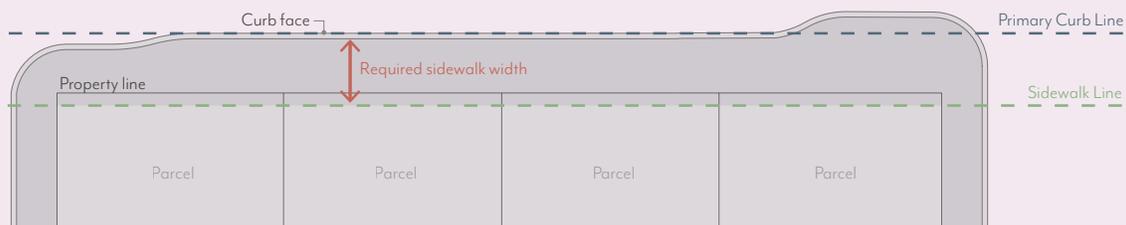
Sidewalks play a multi-faceted role in the built environment, serving as spaces for pedestrian travel, entryways, outdoor dining, landscaping and trees, as well as containing a variety of amenities, such as benches, bus shelters, bicycle racks and trash receptacles. Sidewalk standards correlate to the level of surrounding densities, intensities, and uses. Having sufficient widths and establishing distinct zones ensure that the sidewalk can support activities of all kinds.

- C. **Frontage Zone.** Sidewalks may include a building frontage zone between the Walk Zone and the Sidewalk Line. A maximum width is illustrated in Figures 5.1-3 through 5.1-6.
1. The following elements are permitted within the Frontage Zone and may not encroach on the Walk Zone:
 - a. Seating/furniture,
 - b. Outdoor dining (with a Public Works permit),
 - c. Planters, and/or
 - d. Shade structures and galleries.

Figure 5.1-2: Sidewalk Width Measurement

The sidewalk line is the line created by measuring the required sidewalk width (as shown in Figure 5.1-2) from the Primary Curb Line. The Primary Curb Line is the predominant face of curb line of a given block at the discretion of Public Works, and shall not include “bulb-outs” or reductions in sidewalk width at intersections.

As illustrated here, some parcels may not currently provide sufficient width to meet the sidewalk requirement. In these cases, the property owner must provide additional paved area through a dedication.



SIDEWALK ZONES

The images below reflect examples of appropriate conditions for the three sidewalk zones. These examples are illustrative and may not reflect all applicable development standards.

BUILDING FRONTAGE ZONE



Frontage zones may be used to accommodate outdoor dining



Frontage zones may include planters to add greenery

WALK ZONE



Walk zones of 5 feet allow two people to walk together comfortably



Wider walk zones of 7 feet or more are appropriate for commercial retail areas

AMENITY ZONE



Amenity zones often include landscaped parkways, especially on residential blocks



Amenity zones may include street furniture and landscaping

SIDEWALK DIMENSIONS | CROSS-SECTIONS BY STREET AND ZONING DISTRICT

Figure 5.1-3: CG Sidewalks

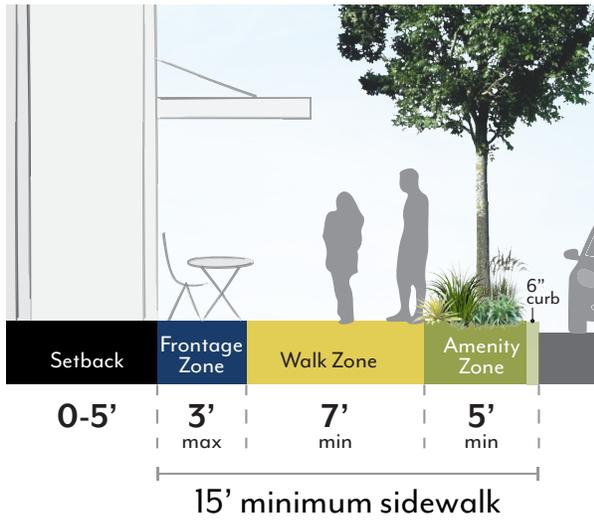


Figure 5.1-4: RM Sidewalks

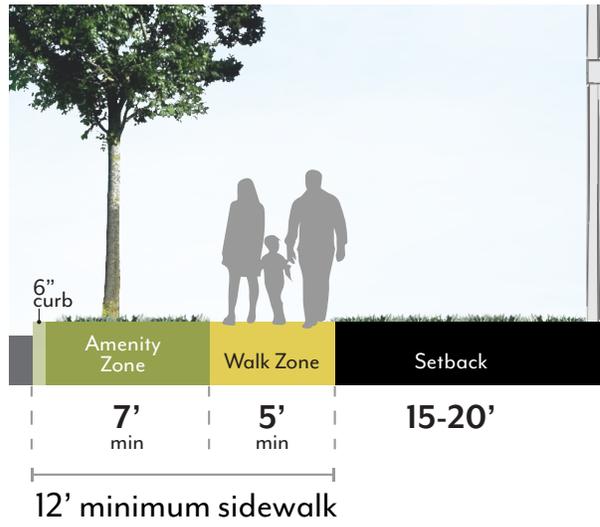


Figure 5.1-5: MU Sidewalks

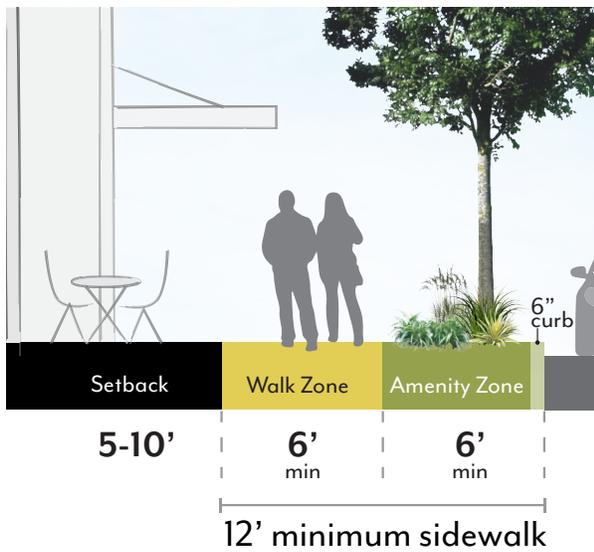
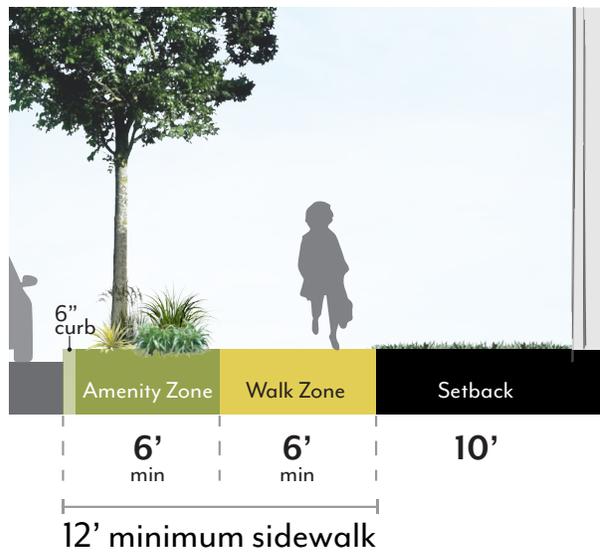


Figure 5.1-6: CF Sidewalks



5.2 Parkways and Street Trees

Standards in sections 5.2 shall apply to all Projects as defined in PMC 17.80.020. These standards are intended to:

- » Enhance pedestrian conditions through increased landscaping at sidewalk level;
- » Provide a visual buffer between parking lane and sidewalk;
- » Improve stormwater capture and increase permeability of sidewalk zone; and
- » Improve street tree health and support the process of carbon sequestration.

5.2.1 PARKWAYS

A. **Required parkways.** Projects shall provide parkways within the amenity zone for between 20 to 30% of parcel frontage. Tree wells shall be counted towards the parkway frontage.

1. Where parkways currently exist, they are permitted to maintain the current parkway frontage even if it is more than 30% of the parcel; however, planted areas shall be updated to meet the requirements in 5.2.1.C.

B. **Dimensions.** Parkway shall be constructed at the same width as the Amenity Zones illustrated in Figures 5.1-3 to 5.1-6, minus the 6-inch width required for the curb. When street parking is adjacent to the curb, an 18-inch paved buffer is required, in addition to the 6-inch curb.

2. The length of individual parkways shall be at least 3 feet and no more than 15 feet. When street trees are planted within a parkway, the minimum parkway length shall be 5 feet.

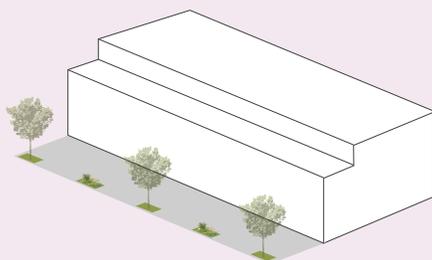
IMPORTANCE OF PARKWAYS

Parkways are landscaped or permeable areas within the sidewalk that play an important role in today's urban landscape by improving pedestrian comfort, increasing sustainability, and enhancing the aesthetic character of the public realm. By expanding the permeable area around street trees, parkways increase rain and stormwater capture, leading to improved street tree health and larger tree canopies, which creates cooler temperatures for pedestrians, helps to sequester carbon from the atmosphere, and reduces pollution in our nearby waterways.

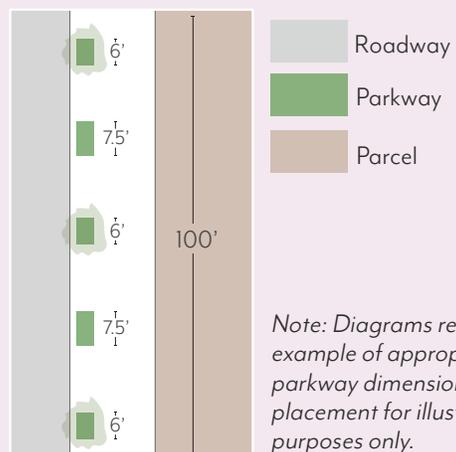
Parkways also provide a visual buffer between the pedestrian and moving or parked vehicles, which further improves pedestrian comfort and creates a more attractive sidewalk environment. Typically residential neighborhoods can accommodate long, uninterrupted areas of parkways within the sidewalk. In commercial and mixed use areas, available space for parkways may be constrained by bus shelters, street lights, and the need to accommodate higher levels of pedestrian traffic; however, significant parkway opportunities still exist on these corridors.

3. Barriers up to 2.5 feet high, such as low walls or fences, are permitted to be constructed at the edge of the parkways but are not required.

Figure 5.2-1: Parkway Requirements



A 20-30% parkway frontage provides room for landscaping and street trees while allowing for other amenities or utilities that may be found in a commercial or mixed-use environment, including seating, waste receptacles, bicycle parking, bus shelters, and driveways.



Note: Diagrams reflect example of appropriate parkway dimensions and placement for illustrative purposes only.

C. **Planted area.** At least 50% of the total parkway area required for a given project must be comprised of plant material.

1. **Materials.** Permitted materials include groundcovers, turf or turf substitutes, and shrubs or low perennials that are lower than 2.5 feet in height at full maturity.

- a. Plant material shall not exceed a height of two 2 feet within 5 feet of a driveway/curb cut.
- b. All plant material shall be native or climate appropriate and have a water use rating of Moderate, Low or Very-Low as defined by Water Use Classification of Landscape Species (WUCOLS) for the region. Plant water use requirements may be relaxed to maximize the efficiency of parkway stormwater capture systems per approval by the Director of Public Works.
- c. Plants with spines or thorns shall not be planted adjacent to any walkways or curbs.
- d. Edible plants are not permitted in parkways.
- e. Artificial turf is not permitted in parkways.

2. **Material removal.** When removing existing plant material like turf grass from a parkway, there shall be no damage to the street tree roots. Parkway improvements involving excavation within an existing tree’s root zone must be consistent with the City’s Tree Protection Guidelines. Root pruning, if required and approved by Public Works, must be overseen by a Certified Arborist. Excavation within a tree’s root zone must be replanted immediately to prevent the tree roots from exposure and undo harm.

D. **Non-vegetative area.** Up to 50% of the parkway area may be organic or inorganic cover.

1. **Materials.** Permitted materials include permeable pavers, decomposed granite, gravel, rocks, or mulch.

- a. Plant Material shall not be placed within 24 inches of tree trunks and shall not run off into the street.
- b. Pavers are not allowed within three (3) feet of any public streetlight pole or pull box or other utility facilities.



Parkway with street trees and low perennial plantings

MATERIALS & ACCESS GUIDELINES

- » In areas with high pedestrian traffic, plant material should have a minimum height of 18 inches to discourage pedestrians from stepping on the parkway. Groundcover is discouraged unless it can withstand heavy foot traffic.
- » Plants which require little or no irrigation are preferred.
- » Plants are not recommended to be planted within 4 feet from a tree trunk.

- E. **Stormwater management.** Parkway shall either meet the following basic stormwater standards, or propose a biofiltration planter or swale design based on local conditions per the approval of the Director of Public Works.
1. **Grade.** The parkway shall be at the same grade as the adjacent hardscape surface at the outer edge of the parkway and slope at a minimum of 1% towards the center of the parkway.
 2. **Required shallow swale.** For parkways with a width greater than 5 feet, the center two feet of the parkway should be depressed 3 to 4 inches to form a shallow swale to collect sidewalk stormwater. Alternative means of storing runoff, such as gravel sumps within the parkway, may be provided.
- F. **Irrigation.** Irrigation systems in parkways must be designed and constructed in a manner that will eliminate surface runoff onto any impermeable surface, public or private, under any condition. Design of irrigation systems in parkways shall be in accordance with all local, state, and federal laws and regulations for water conservation. Street tree roots shall not be damaged during the irrigation installation process.
- G. **Maintenance.** Abutting property owner shall ensure and maintain the parkway in a condition so as not to endanger persons or property, and not to interfere with the public convenience.

STORMWATER & IRRIGATION GUIDELINES

- » Parkway should be designed to treat and/or capture stormwater run-off from the adjacent to the greatest extent feasible given soil conditions.
- » Suspended pavement systems are encouraged as a means of controlling runoff volume and should be implemented under and adjacent to large pedestrian walkways.
- » If impermeable surfaces are used within parkways, they shall be constructed to drain to permeable areas.
- » Low-volume, sub-surface/drip irrigation or other non-spray irrigation systems or hand-watering is preferred where irrigation is needed.

5.2.2 STREET TREES

- A. **Tree species.**¹ Street tree species shall be selected according to the Master Street Tree Plan at the discretion of the Director of Public Works. Trees may be planted within parkways or tree wells.
- B. **Tree spacing.** Street trees shall be planted at a spacing no greater than one per every 30 feet. Exceptions can be made by the Director of Public Works due to conflicts with street lights, bus shelters, utility boxes, or other street amenities. Closer spacing is encouraged when feasible and when appropriate for the particular tree type.
- C. **Tree well dimension.** Tree well width must be equivalent to the required Amenity Zone, minus the 6-inch width required for the curb. If a paved buffer zone is required due to adjacent street parking, the tree well width may be reduced to accommodate this buffer strip. The minimum length of a tree well shall be 6 feet. Street trees planted within tree wells must be installed according to the Department of Public Works Tree Planting in Tree Well Standard Plan.
- D. **Tree well frames.** Tree well frames, or tree grates, may be installed according to the Department of Public Works Tree Well Frame Installation Standard.
- E. **Expanded Root Zone Cell.** Each street tree shall be provided with an uncompacted root zone volume of at least 800 cubic feet. The root zone volume depth shall be 2 feet minimum and 3 feet maximum. Where this root zone volume cannot be provided within the parkway area, an expanded root zone cell volume shall be provided below adjacent pavement using a strategy such as structural soil or a suspended pavement system to provide an uncompacted soil area suitable for tree root growth. The root zone volume per tree requirement may be reduced by 10% where two or more trees share a contiguous root zone cell.
- F. **Maintenance.** All street trees shall be maintained by the Department of Public Works.

¹ See **Appendix A.2 Design Guidance for Tree Selection** for detailed recommendations to better align Lincoln Avenue’s street tree species with the vision, goals, and policies in this Specific Plan related to shade, climate resilience, stormwater capture, and supporting a vibrant public realm.

IMPORTANCE OF STREET TREES

Street trees play an important role in keeping cities livable, sustainable and resilient. Trees improve air quality, increase urban biodiversity, and help reduce carbon emissions. In addition to environmental benefits, trees provide health, social, economic, and aesthetic benefits to communities. Requirements based on guidance from the City’s Master Street Tree Plan will increase street tree coverage and require the preservation or introduction of certain tree species. In adherence with these street tree standards and guidelines, new development will contribute to an enhanced shade canopy that helps to reduce the urban heat island effect, decrease sidewalk temperatures, enhance pedestrian comfort, and improve the visual experience of the street.



Young Oak tree on Lincoln Ave.



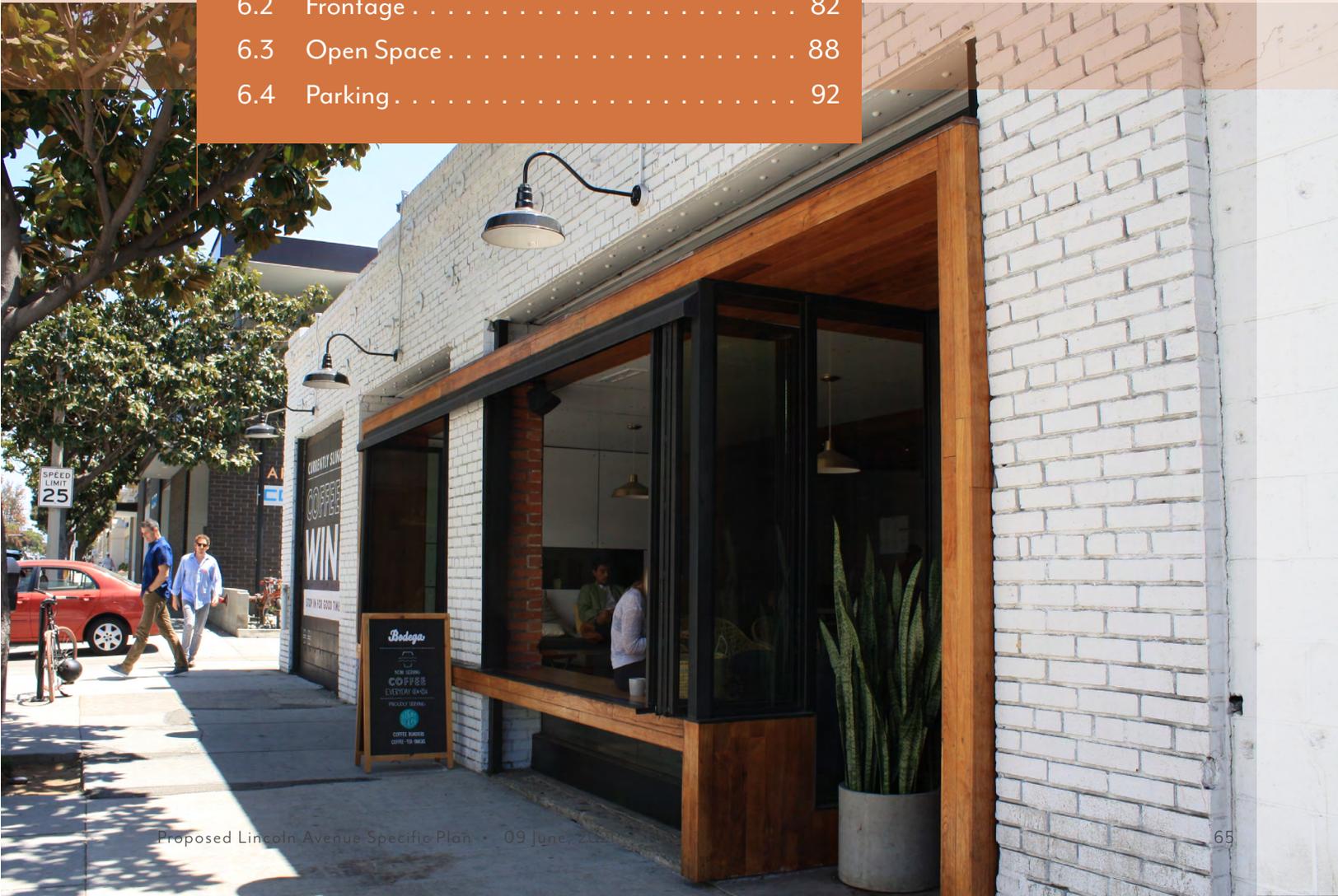
Mature Oak tree on Lincoln Ave.

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Ch. 6

Development Standards

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Development Standards

CHAPTER OVERVIEW

The development and design standards in this chapter serve to implement the vision, goals, and policies for the Lincoln Avenue Specific Plan area, described in Chapter 3.

This chapter is organized into the following sections:

- » **6.1 Scale.**
 - » 6.1.1 Density (du/ac)
 - » 6.1.2 Intensity (FAR)
 - » 6.1.3 Height*
 - » 6.1.4 Setbacks*
 - » 6.1.5 Stepbacks*
 - » 6.1.6 Historic Adjacency*
 - » 6.1.7 Modulation
- » **6.2 Frontage.**
 - » 6.2.1 Ground Floor
 - » 6.2.2 Entrances
 - » 6.2.3 Transparency
 - » 6.2.4 Shade Structures
 - » 6.2.5 Arcades & Galleries
 - » 6.2.6 Lighting
 - » 6.2.7 Walls & Fences*
 - » 6.2.8 Balconies & Roof Decks*
- » **6.3 Open Space.**
 - » 6.3.1 Minimum Area
 - » 6.3.2 Private Open Space
 - » 6.3.3 Common Open Space
 - » 6.3.4 Public Open Space
- » **6.4 Parking.**
 - » 6.4.1 Minimum Parking*
 - » 6.4.2 Vehicle Access
 - » 6.4.3 Layout & Design

*Applicable RM-16 standards modified by this Specific Plan.

In addition to the requirements of this Specific Plan, all projects shall comply with the Pasadena Municipal Code (PMC) requirements below. In the event of conflict between the Zoning Code and this Specific Plan, the requirements of this Specific Plan shall control (PMC 17.12.020.D).

- » PMC 17.40 General Development
- » PMC 17.42 Inclusionary Housing
- » PMC 17.43 Density Bonus
- » PMC 17.44 Landscaping
- » PMC 17.46 Parking & Loading
- » PMC 17.48 Signs
- » PMC 17.50 Specific Land Uses

In LASP RM-16, development shall follow all RM-16 standards in PMC 17.22 except where modified by this Specific Plan.

Per Section 4.1.2, standards for PS zoning are not included in this Specific Plan. In PS, development shall be subject to a Conditional Use Permit or Master Plan (PMC 17.26).

Guidelines, incorporated as part of this plan in shaded text boxes, are intended to encourage quality architecture that enhances the community's unique character. Projects should also consult Pasadena's *Design Guidelines for Neighborhood Commercial and Multi-Family Districts* for further guidance on building form and relationship to the surrounding neighborhood. Projects required to go through Design Review will be assessed based its contents.

Table 6-1: Summary of Development and Design Standards

Table 6-1 provides abbreviated development and design standards by zoning district for the Lincoln Avenue Specific Plan. Where the Plan defers to the Pasadena Municipal Code (PMC) for a particular standard, the relevant code section is provided; however, the city's code is updated

periodically and exact code references may change. Checkmarks (✓) indicate where a Specific Plan standard applies, but the standard is text-based and cannot be condensed into the table. **Complete standards shall be referenced within the relevant sections of Chapter 6.**

Standard	LASP-CG	LASP-CL	LASP-CF	LASP-MU	LASP-RM-16
Scale					
Maximum Density					
Dwelling Units per Acre	0	0	0	48	16
Maximum Intensity					
Floor Area Ratio	1.0	1.0	1.0	1.5	N/A
Maximum Height					
Height (stories)	39'	36'	39'	39' (3)	36'
Required Setbacks					
Lincoln Avenue	0-5' for 75% of frontage	15'	5-10' for 75% of frontage	5-10' for 75% of frontage	15-20' for 50% of frontage
Other streets	0-5' for 75% of frontage	None required	5-10' for 50% of frontage	5-10' for 50% of frontage	10'
Adjacent RM/RS	15'	15'	N/A	15' (5' if Res.)	5'
Other interiors	None required				5'
Required Stepbacks					
All streets	None required			10' deep at 39'	None required
Adjacent RS	Figure 6.1-5	Figure 6.1-3	None required	Figure 6.1-3	Figure 6.1-4
Historic Adjacency	✓	✓	✓	✓	✓
Setbacks & Stepbacks	Modified standards apply to Projects adjacent designated resources				
Required Modulation					
Length	100'	75'	150'	150'	PMC17.22.070
Area	25%	25%	25%	25%	
Frontage					
Minimum Ground Floor Dimensions					
Height	15'	15'	15'	15'	PMC17.22.070
Non-residential Depth	35'	35'	35'	35'	
Entrances	✓	✓	✓	✓	
Minimum Transparency					
Ground Floor	70%	30%	30%	70%	N/A
Overall Façade	30%	15%	15%	30%	15%
Residential Units	N/A	N/A	N/A	15%	15%

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Standard	LASP-CG	LASP-CL	LASP-CF	LASP-MU	LASP-RM-16
Arcades/Galleries	✓	✓	✓	✓	N/A
Required Shade Structures	✓	N/A	N/A	N/A	
Required Lighting	✓	N/A	N/A	N/A	
Walls & Fences	✓	✓	✓	✓	✓
Balconies & Roof Decks	✓	✓	✓	✓	✓
Open Space					
Minimum Area	✓	✓	✓	✓	PMCI7.22.080
Private Open Space	✓	✓	✓	✓	
Common Open Space	✓	✓	✓	✓	
Public Open Space	✓	N/A	✓	N/A	
Parking					
Minimum Parking	✓	✓	✓	✓	✓
Vehicle Access	✓	✓	✓	✓	PMCI7.22.070
Layout & Design	✓	✓	✓	✓	
Other Applicable Standards¹					
City of Gardens	N/A	N/A	N/A	N/A	PMCI7.22.070
General Development	PMCI7.40				
Inclusionary Housing	PMCI7.42				
Density Bonus	PMCI7.43				
Landscaping	PMCI7.44				
Parking & Loading	PMCI7.46				
Signs	PMCI7.48				
Specific Land Uses	PMCI7.50				

¹ Projects shall follow all requirements below except where modified by this Specific Plan. In the event of conflict between the Zoning Code and this Specific Plan, the requirements of this Specific Plan shall control, per PMC 17.12.020.D.

6.1 Scale

These standards are intended to:

- » Implement the General Plan density (du/ac) and floor area ratio (FAR) values;
- » Shape development in a manner that creates a defined public realm and appropriate scale of buildings for a visually appealing community;
- » Reduce building massing through setback and stepback requirements that create appropriate transitions to residential neighborhoods;
- » Support high-quality architecture and urban design through modulation requirements and variation in façade length; and
- » Require appropriate transitions to designated historic resources.

6.1.1 DENSITY

- A. **Residential Density.** Projects that include residential dwelling units shall not exceed the allowable dwelling units per acre (du/ac) set in Map 6.1-1.
1. Fractions shall be rounded to the nearest whole number; those at 0.50 may be rounded up. For projects utilizing state density bonus, refer to Government Code 69515.
 2. This maximum is based on gross site area; a reduction in buildable area due to dedications/easements shall not reduce allowable maximums.

Map 6.1-1: Maximum Density



Map 6.1-2: Maximum Floor Area Ratio



6.1.2 INTENSITY

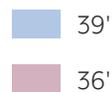
- A. **Floor Area Ratio.** Projects that include non-residential space, including mixed-use, shall not exceed the allowable floor area ratio (FAR) set in Map 6.1-2.
1. In mixed-use projects, residential floor area is included in FAR.
 2. Areas used exclusively for vehicle and bicycle parking and loading are excluded from FAR.
 3. This maximum is based on gross site area; a reduction in buildable area due to dedications/easements shall not reduce allowable maximums.

Map 6.1-3: Maximum Building Height



6.1.3 HEIGHT

- A. **Building Height.** Projects shall not exceed the height limits set in Map 6.1-3.
 - 1. Exceptions allowed for projecting features such as appurtenances and railings per PMC 17.40.060.



SAMPLE HEIGHTS

Maximum heights of 36' and 39' allow for up to 3 stories in building height. Architectural features such as roof shape, stepbacks, and modulation can add variation to the roof line and reduce visual impact of building height. These examples are illustrative and may not reflect all applicable development standards.



Example of approximately 36' building



Example of approximately 39' building

SAMPLE SETBACKS

Street setbacks refer to the space between the public sidewalk and a building. Setback standards create a consistent streetwall and help achieve an appropriate level of interaction between the public realm and private properties. These examples are illustrative and may not reflect all applicable development standards.



Example of approximately 5' setback

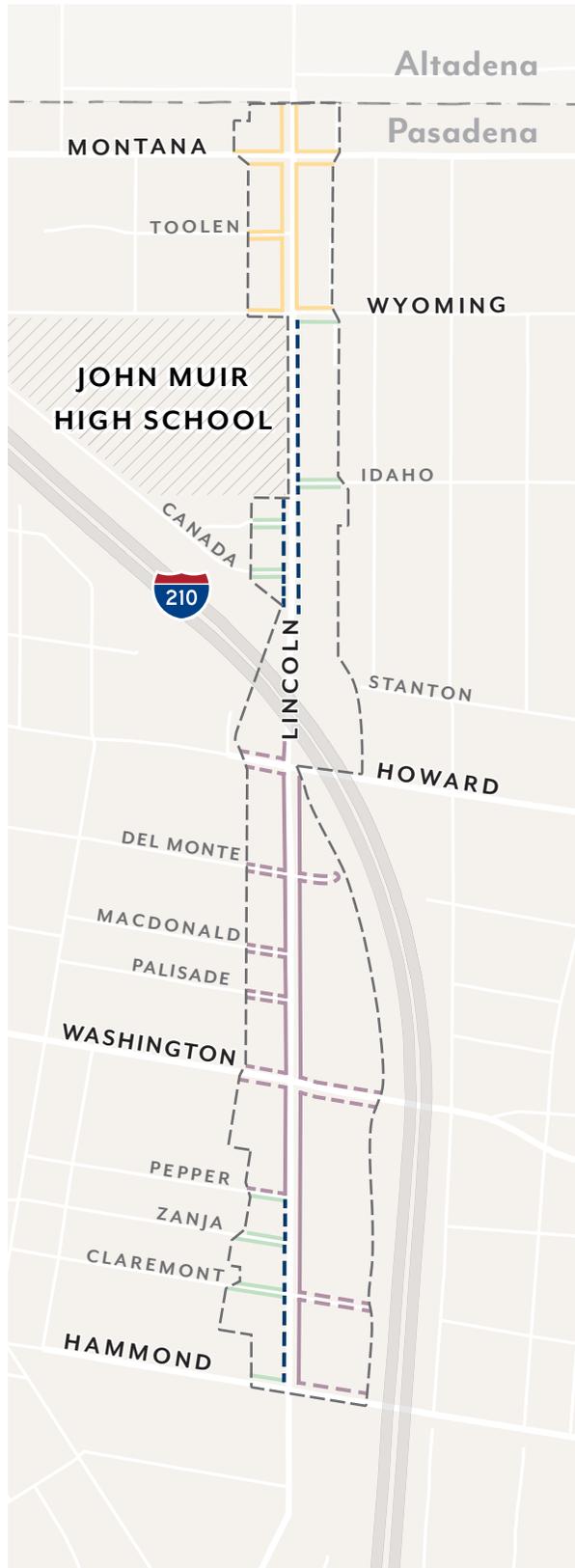


Example of 5-10' setback



Example of 15 - 20' setback

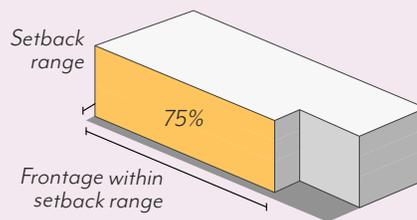
Map 6.1-4: Street Setbacks



6.1.4 SETBACKS

- A. **Street Setbacks.** Projects shall comply with the street setback ranges set in Map 6.1-4. Ranges establish a minimum and maximum for the specified percentage of frontage; see Figure 6.1-1. While the remaining frontage may exceed the maximum, setbacks less than the minimum are prohibited.
1. Street setbacks are measured from the sidewalk line; see Figure 5.1-2.
 2. Exceptions allowed per PMC 17.40.160 (Table 4-1).
 3. Arcades and recessed ground floors up to 15 feet in depth are allowed when a second story meets the specified setback.
 4. Other features allowed within the street setback include:
 - a. Landscaping and planters;
 - b. Hardscape (e.g. stoops, patios);
 - c. Shade structures per Section 6.2.4;
 - d. Arcades and galleries per Section 6.2.5;
 - e. Walls and fences per Section 6.2.6;
 - f. Seating and furniture; and
 - g. Other open space amenities per review authority approval.

Figure 6.1-1: Setback Range



Up to 25% of building can be set back further than the range (percentage varies by street)

Note: Diagram used for illustrative purposes only.

- | | |
|---|--|
| — 0-5' for 75% | - - - 15-20' for 50% |
| - - - 5-10' for 75% | - - - 5' minimum |
| - - - 5-10' for 50% | |

- B. **Interior Setbacks.** Projects shall comply with the interior setbacks set in Table 6.1-1 based on project type when adjacent to residential zoning. Interior setbacks are not required along other property lines.
1. Interior setbacks are those abutting other parcels (non-street side and rear) and are measured from the shared property line.
 2. Exceptions allowed per PMC 17.40.160 (Table 4-1).

Table 6.1-1: Interior Setbacks Adjacent to RM/RS

Project type	CG, CL, CF	MU	RM-16
Mixed-use	15' min.		N/A
Nonresidential	15' min.		N/A
Residential	N/A	5' min.	

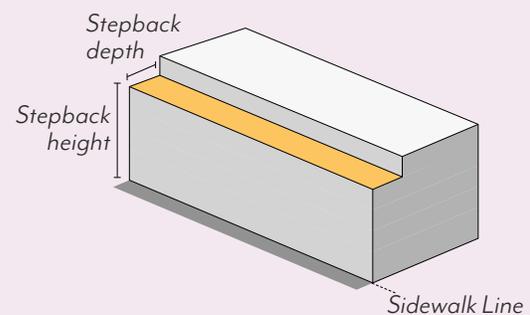
6.1.5 STEPBACKS

- A. **Street Stepbacks.** Projects utilizing a height concession per PMC 17.43.055 shall provide a stepback depth of at least 10 feet at a height of 39 feet or less along street frontages; see Figure 6.1-2.
1. Street stepbacks are those abutting public right-of-way and are measured from the sidewalk line.
 2. Uses allowed within the street stepback include:
 - a. Open Space (e.g. balconies, terraces);
 - b. Shade structures, trellises, and similar;
 - c. Green roofs and photovoltaic panels; and
 - d. Other open space features per review authority approval.

- B. **Interior Setbacks.** Adjacent to RS zoning districts, projects shall comply with the stepbacks below. Interior setbacks are not required along other property lines.
1. In all zones except LASP-RM-16, projects shall not be located within the encroachment plane sloping upward and inward at a 45-degree angle measured from the vertical, commencing 15 feet above the existing grade along the shared property line; see Figures 6.1-3 and 6.1-5.
 2. In LASP-RM-16, projects shall step back 15 feet at the second story and 50 feet at the third story, measured from the shared property line; see Figure 6.1-4.
 3. Exceptions allowed per PMC 17.40.160 (Table 4-2.1).

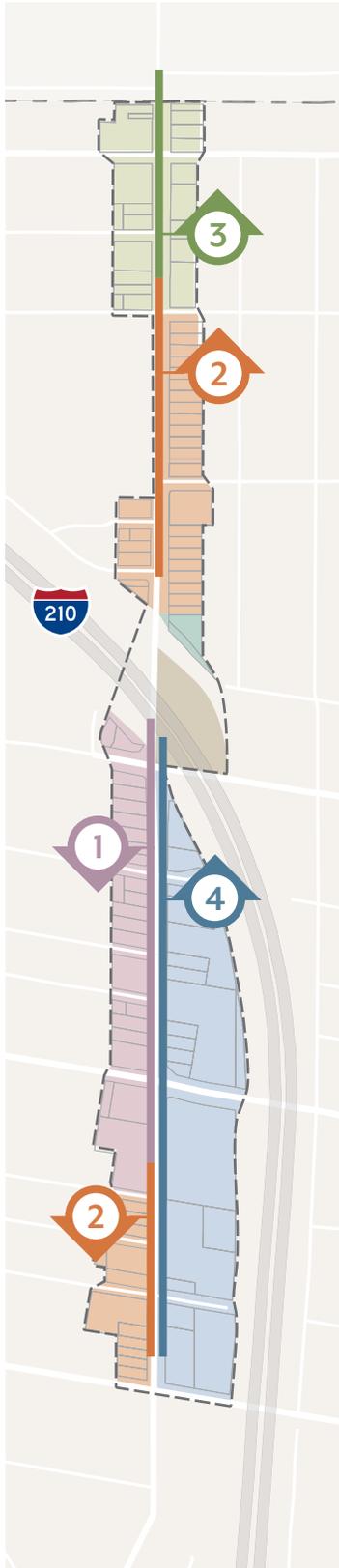


Figure 6.1-2: Street Stepbacks



Note: Diagrams used for illustrative purposes only.

Map 6.1-5: Sections Key



BUILDING ENVELOPES | CROSS-SECTIONS BY ZONING DISTRICT

The colored bars and numbered labels correspond to the sections in Figures 6.1-3 through 6.1-6. The arrows indicate the direction of the views represented.

Figure 6.1-3: LASP-MU Buildings

1

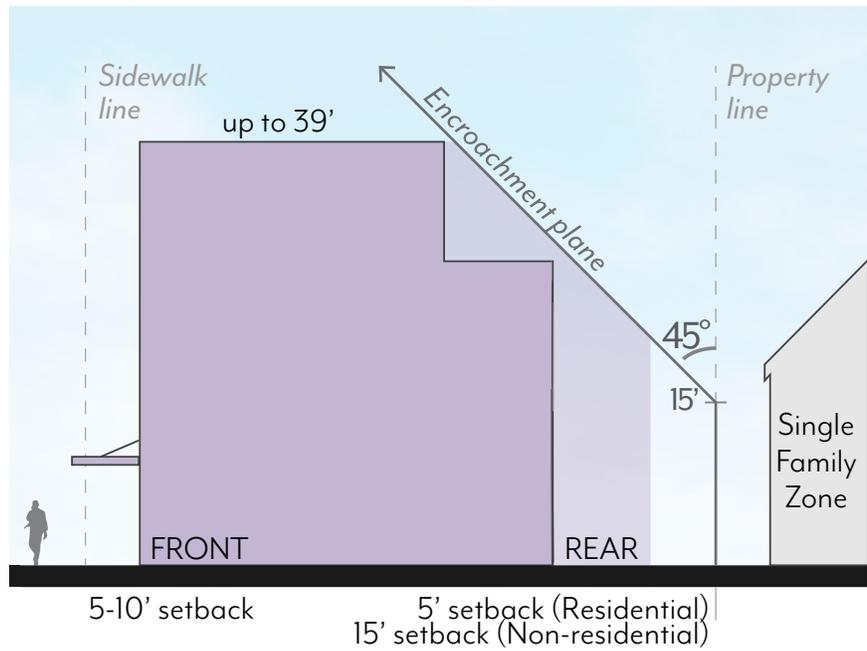


Figure 6.1-4: LASP-RM-16 Buildings

2

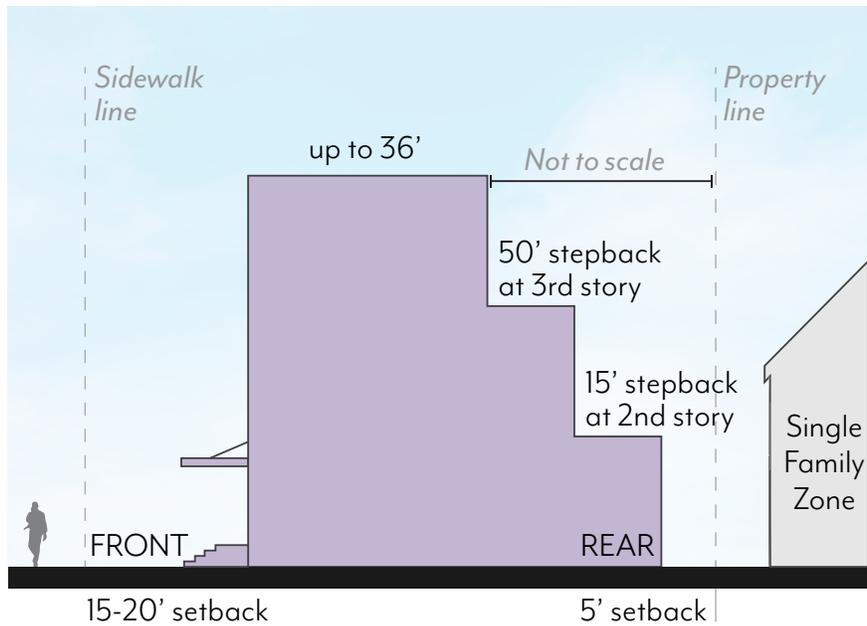


Figure 6.1-5: LASP-CG Buildings

3

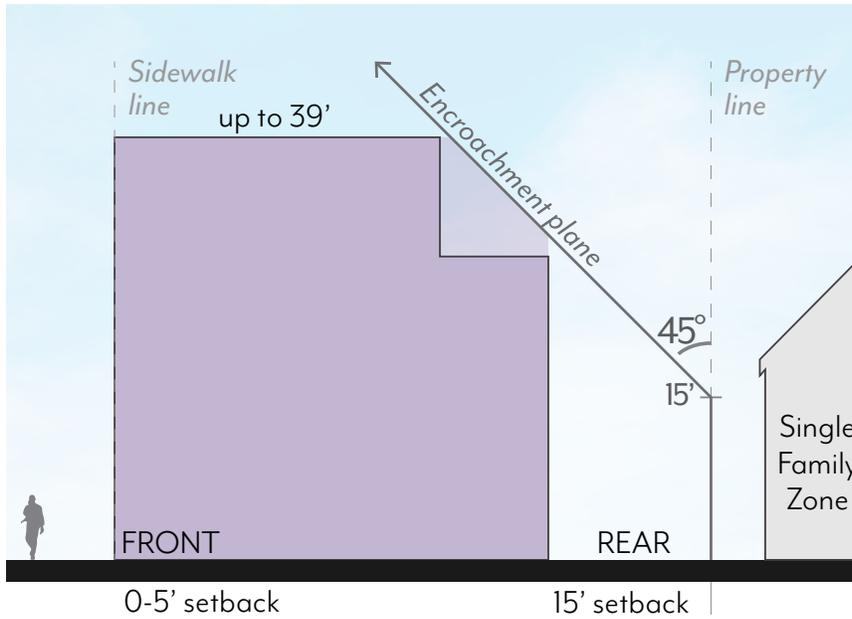
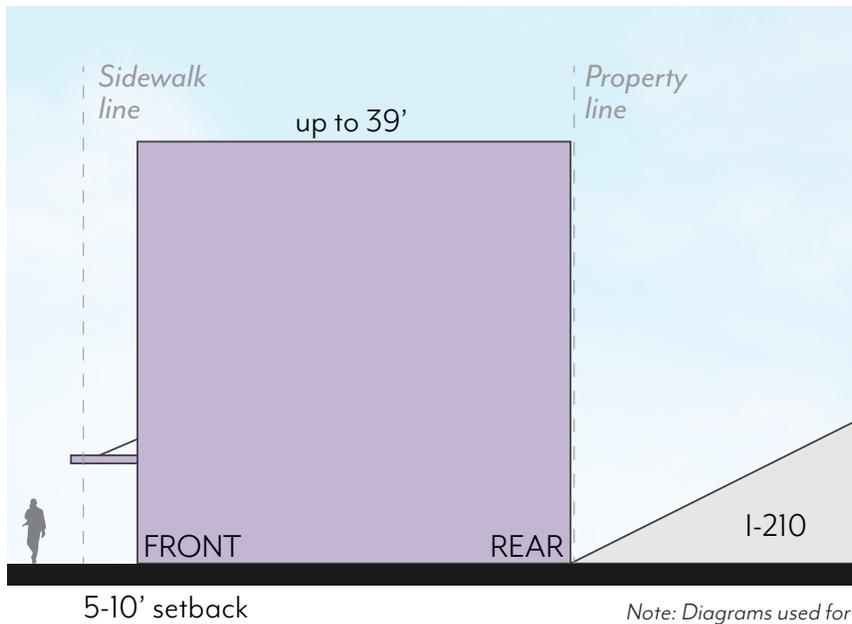


Figure 6.1-6: LASP-CF Buildings

4



Note: Diagrams used for illustrative purposes only.



Dental Center Building at 2030 Lincoln Ave.



Kettle's Nursery Building at 1960 Lincoln Ave.

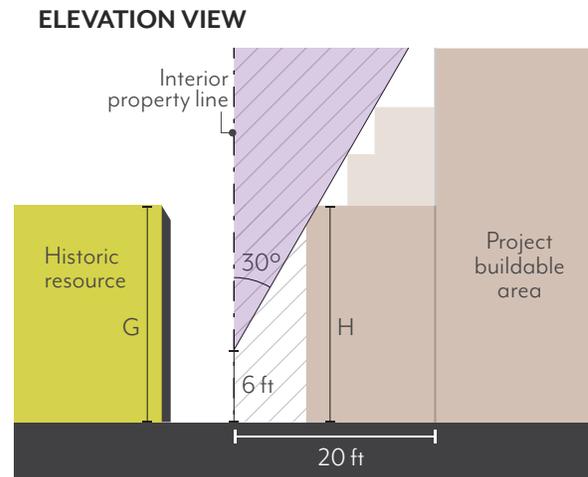
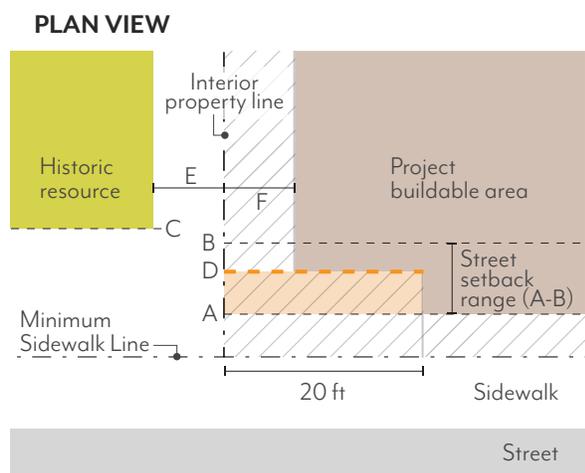
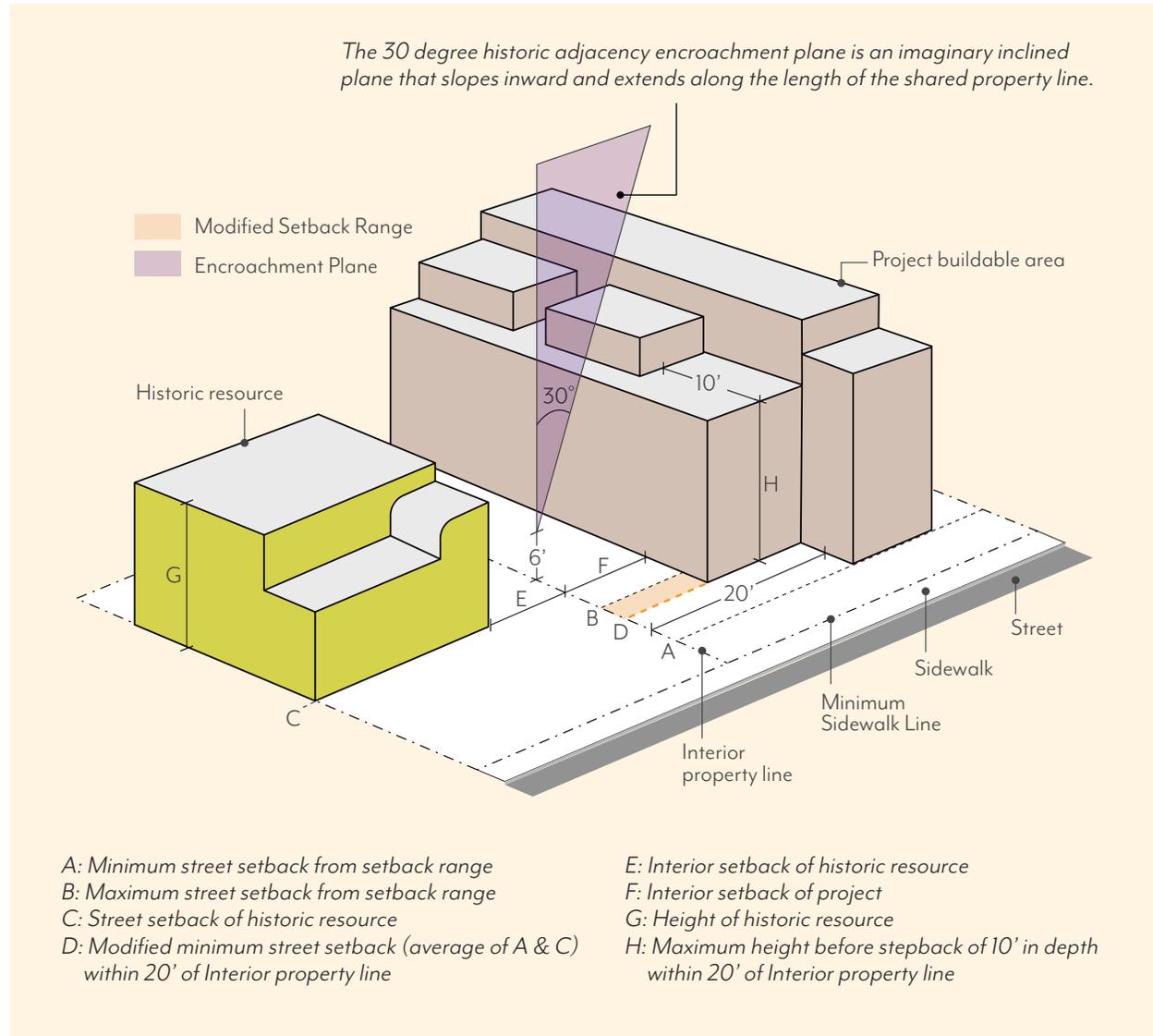


California Bungalow style house at 1826 Lincoln Ave.

6.1.6 HISTORIC ADJACENCY

- A. **Landmark Properties.** Projects on parcels with a designated historic resource shall be subject to review for consistency with the Secretary of the Interior's Standards and approval by the Design Commission.
- B. **Transition Massing.** Projects sharing a property line with a designated historic resource (resource) are subject to the following modified standards, illustrated in Figure 6.1-7.
 1. **Street Setbacks:** The minimum street setback shall be an average of the minimum setback set in Map 6.1-4 and the established setback of the resource for at least 20 feet from the shared property line. If between two resources, the street setback shall be an average of the setbacks of the two resources for the full street frontage.
 2. **Interior Setbacks:** The minimum interior setback shall be equal to that of the historic resource. No setback is required where the resource is built to the shared property line.
 3. **Interior Stepbacks:** Projects shall not be located within an encroachment plane sloping upward and inward at a 30-degree angle measured from the vertical, commencing 6 feet above the existing grade at the property line; see Figure 6.1-7. This plane is not applicable if the resource is built to the shared property line.

Figure 6.1-7: Historic Adjacency Transition Massing



Note: Diagrams used for illustrative purposes only.

Historic Legacy: 1960 Lincoln Avenue

The building at **1960 Lincoln Avenue** represents early Googie-style architecture that became popular after World War II as America was transformed by futurism and car culture. Typical roadside architecture evolved with bright colors, oversized signage, and exaggerated forms.

The former **Foster's Old Fashion Freeze** served up "California's Original" soft-serve ice cream from about 1948 to 1968. In the late 1960s when plans for Interstate 210 displaced businesses further south on Lincoln Avenue, **Kettle's Nursery** relocated and took over the property. A family-owned Pasadena business from the late 1940s, it's operated on the site since 1970.

The style was widely used on coffee shops, gas stations, motels, and restaurants through the 1950s and 1960s. However, the commercial nature of these buildings generally involves frequent tenant changes and remodels, which have made intact examples rarer. The building retains primary features of the original design that convey its mid-century style and feeling, including its wide-overhanging roof with round corners, rooftop metal sheet signage (modified), slanted plate glass walls, and stone siding.

DESIGN GUIDELINES

- » **Access.** The streetscape at the primary façade on Lincoln Avenue should maintain a pedestrian-oriented walkway for at least the appearance of access to the walk-up service windows in the primary façade.
- » **Alterations.** Changes to the building should repair and maintain its primary features to perpetuate its early Googie-style feeling. Restoration of signage, walk-up windows, or other removed features should be based on historic examples so as not to create a false sense of history for the building. Interior changes to the building should avoid blocking the transparency of the storefront.
- » **Architecture.** New development should consider the Modernist style and materials of the building for a complementary aesthetic.
- » **Site.** A larger project on the site may incorporate the building into a larger floorplan but should respect the standalone nature of the building by implementing a 15-foot buffer of open space around the extent of the roofline perimeter at the front (west) and side (north and south) elevations. Any new connections to the structure itself should be restricted to the rear.



6.1.7 MODULATION

- A. **Façade Length.** Buildings that exceed the length set in Table 6.1-2 along any street frontage shall include a break of at least 10% of the façade length or 20 feet, whichever is more. This break shall be at least 10 feet deep, open to the sky; see Figure 6.1-8.

Table 6.1-2: Modulation

Façade	CG	CL	CF	MU
Length	100'	75'	150'	150'
Area	25%	25%	25%	25%

- B. **Façade Area.** Street-facing façades over 50 feet in length shall modulate a minimum of 25% of the area above the ground floor between 2 feet and 12 feet in depth from the primary façade plane; see Figure 6.1-9. Buildings with 2 stories or less are exempt.
1. The primary façade plane is defined as the vertical plane with the greatest surface area above the ground floor.
 2. Modulation is not required to be continuous or open to the sky, and may be recessed or projected, but not past the property line.
 3. Required stepbacks (6.1.5.A), façade breaks (6.1.7.A), and projected balconies (6.2.8.A) shall not count toward the modulation requirement; balconies that are recessed at least 2 feet shall count.

CREATING VISUAL INTEREST AND BREAKING UP MASS

Façade modulation refers to variations in depth of a building's façade. Modulation can be achieved through architectural elements such as fenestration patterns and window bays, stepbacks, balconies, and full façade breaks. Modulation breaks up building massing, creates visual interest, and provides opportunities for open space.

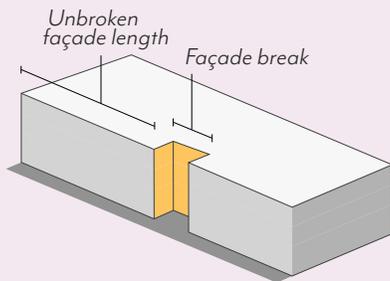


Façade plane breaks help to prevent monolithic building forms



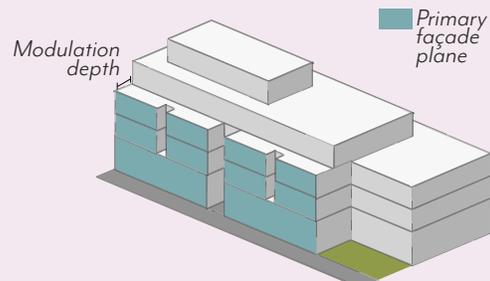
Balconies and other variations in wall plane depth create visual interest

Figure 6.1-8: Façade Length



Façades over the specified length shall include a break at least 20 feet wide and 10 feet deep, open to the sky.

Figure 6.1-9: Façade Area



Façades shall modulate at least 25% of the area above the ground floor between 2 and 12 feet in depth.

Note: Diagrams used for illustrative purposes only.

6.2 Frontage

These standards are intended to:

- » Prioritize pedestrian access by ensuring doorways are open to a public sidewalk or public open space;
- » Increase visibility into ground floor uses to create visual interest for pedestrians;
- » Promote shade through arcades and shade structures;
- » Support a consistent character when different uses are allowed on the ground floor within the same block; and
- » Limit blank walls on the ground floor to enhance visual interest and pedestrian comfort.

6.2.1 GROUND FLOOR

- A. **Height.** Buildings shall have a minimum ground floor height of 15 feet, measured from sidewalk elevation to second story floor.
1. For residential units, floors may be elevated up to 6 feet above the sidewalk elevation. Where elevated between 4 and 6 feet, a minimum setback of 8 feet is required.
- B. **Depth.** Commercial uses facing the street shall have a minimum depth of 35 feet for at least 80% of the commercial frontage.

Figure 6.2-1: Ground Floor Height

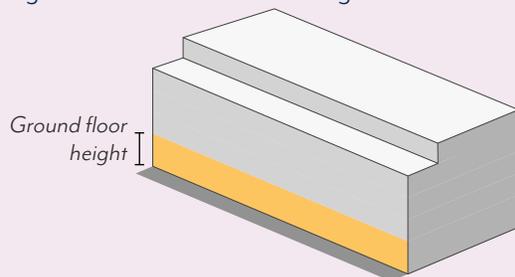
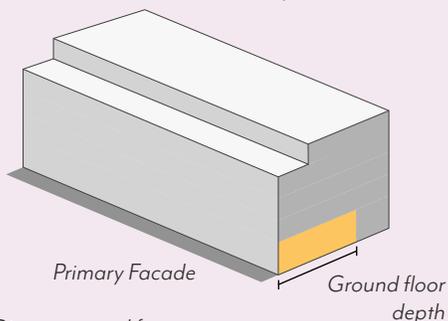


Figure 6.2-2: Ground Floor Depth



Note: Diagrams used for illustrative purposes only.

CREATING A VIBRANT STREET ENVIRONMENT

Successful ground floor design contributes to a vibrant built environment to create an inviting, visually engaging, shaded sidewalk and pedestrian environment that supports commercial activity. Altogether, ground floor treatments increase visual interest and physical access across all building uses to create active frontages.



A commercial ground floor of at least 15 feet in height



A residential ground floor elevated above the sidewalk

6.2.2 ENTRANCES

- A. **Location.** At least one primary entrance shall be on the primary frontage of each building and open into the public realm (e.g. a sidewalk or public open space).
 1. In LASP-CG, entrances shall be recessed at least 30 inches from the building façade along Lincoln Avenue. On other streets, all entrances shall be recessed at least 30 inches from the sidewalk line.
 2. Primary entrances shall be distinguished by architectural features such as overhead projections (e.g. an awning or canopy).
 3. For non-residential uses, primary entrances shall be located at sidewalk elevation.



Recessed entrance with overhead awnings

6.2.3 TRANSPARENCY

- A. **Windows & Doors.** Minimum transparency for street-facing façades is set in Tables 6.2-1 and 6.2-2 based on use.
 1. For non-residential and residential common space uses, ground floor transparency is measured as the percentage of building frontage that consists of transparent openings between a height of 2 feet and 10 feet above sidewalk elevation.
 2. All other transparency is measured as the percentage of building frontage area, viewed in elevation.
 3. Windows shall be recessed by at least 2 inches from the façade; flush windows may be allowed per review authority approval.
 4. The use of tinted, mirrored or highly reflective glazing is prohibited.
 5. Blinds, drapes, posters, and shelving for product displays visible to the public right-of-way shall obscure no more than 10 percent of the transparent areas of each respective storefront.
- B. **Blank Walls.** Windowless expanses of street-facing walls shall not exceed 20 feet in length.
- C. **Security Bars.** Any exterior or interior security bars shall be designed to be fully hidden from view during business hours with devices such as concealed side pockets and ceiling cavities.



Recessed entrance with ground floor transparency

Table 6.2-1: Transparency for Non-residential Uses and Residential Common Space

Non-residential	CG	CL	CF	MU
Ground Floor	70%	30%	30%	70%
Overall Façade	30%	15%	15%	30%

Table 6.2-2: Transparency for Residential Units

Residential units	CG	CL	CF	MU
Ground Floor		N/A		15%
Overall Façade		N/A		15%

DESIGN GUIDELINES FOR COMMERCIAL BUILDINGS

New development along Lincoln Avenue should complement and respond to the immediate area, reflecting the scale and proportion of existing commercial buildings in the corridor.

These are predominantly one-story buildings with rows of typical early to mid-20th century storefronts composed of plate glass windows and recessed glazed doors that have minimal setbacks from the sidewalk.

- » **Access.** The streetscape should maintain pedestrian-oriented views of the storefronts/primary façades. No fencing should obscure the façades, which should remain approachable.
- » **Alterations.** Changes to an existing building should repair and maintain its primary features, including traditional materials, such as brick and stucco, and storefront configurations, including large expanses of windows and main entrances oriented towards the sidewalk. Upper story additions should respond to the existing cornice or roofline facing the sidewalk and use complementary fenestration patterns and materials.
- » **Articulation.** New building design within the commercial corridor should implement façade patterns that modulate the appearance of a traditional storefront (approximately 25 to 50 feet wide) to promote the sense of a variety of small establishments.

Refer to Pasadena's *Design Guidelines for Neighborhood Commercial and Multi-Family Residential Districts* for further detail.



Commercial building on Lincoln Avenue with recessed entrance and transparency



Commercial building on Lincoln Avenue using traditional materials and facade patterns.



Facade transparency adds to the public realm by creating a visual interaction between businesses and the adjacent sidewalk

DESIGN GUIDELINES FOR COMMERCIAL BUILDINGS

- » **Storefront.** Ground floor treatments should be pedestrian in scale, character, and design detail. Building elements such as lighting, signage, and awnings should be coordinated to contribute to the continuity of pedestrian scale, storefront character, and street activity.
- » **Lighting.** Exterior light fixtures should be designed to illuminate the sidewalk, building entrances, storefront signage, and architectural features of the building. Fixtures should be simple and unobtrusive in appearance and size. Do not over-light the building with high intensity lighting or project light into the sky.
- » **Signage.** Signs should be oriented toward the pedestrian and designed to be read from a distance of 15 to 20 feet. Signs should be mounted in locations that respect the design of a building and be sized to fit; they should not cover windows or architectural features. Projecting blade signs are highly encouraged.
- » **Awnings.** Awnings should be mounted over individual bays of a storefront; those that extend over masonry piers and transom windows are strongly discouraged. Shed awnings, with no end panels and simple horizontal valances, are the preferred awning style.

Refer to Pasadena’s *Sign Design Guidelines* for further detail.



Commercial building with gooseneck lighting at a pedestrian height



Pedestrian-oriented commercial storefront with awning



Blade signs are oriented to the pedestrian for easy visibility and recognition

6.2.4 SHADE STRUCTURES

- A. **Shading.** In LASP-CG along Lincoln Avenue, shade structures (e.g. awnings and canopies) are required and shall project at least 5 feet and no more than 10 feet into the public right-of-way for at least 50% of the building frontage. For all other frontages, shade structures may project up to two-thirds of the sidewalk width.
1. Shade structures shall allow at least 8 feet of vertical clearance from sidewalk elevation and shall not conflict with existing trees.
 2. Where an arcade or recessed ground floor provides at least 5 feet of unobstructed pedestrian clearance, shade structures are not required.

6.2.5 ARCADES & GALLERIES

- A. **Arcades.** Arcades may be located in street setback ranges, but shall not encroach past the sidewalk line.
1. Any arcades shall be at least 8 feet from back of column to building façade. The distance between columns shall be at least the same dimension as arcade depth, as measured from the column center.
 2. The façade shall still meet the ground floor transparency set in Section 6.2.3.
 3. Uses allowed within arcades include:
 - a. Pedestrian travel,
 - b. Seating/street furniture,
 - c. Outdoor dining,
 - d. Landscape planters, and/or
 - e. Bicycle parking.
- B. **Galleries.** Galleries may be located in street setbacks, and those up to 50 feet in length may project over public right-of-way with approval from Design Commission and Public Works.
1. Galleries shall allow at least 8 feet of vertical clearance from sidewalk elevation
 2. Columns shall not be located in the Amenity Zone. The minimum Walk Zone dimension shall be maintained between columns and the sidewalk line.
 3. Sidewalks shall still meet parkway requirements set in Section 5.2.

TRANSITIONAL OUTDOOR SPACES

Arcades create a shaded outdoor space between the public realm and ground floor interior. Shade structures contribute to a comfortable pedestrian experience and serve as inviting design features for ground floor uses.



Arcades provide shaded space for pedestrians



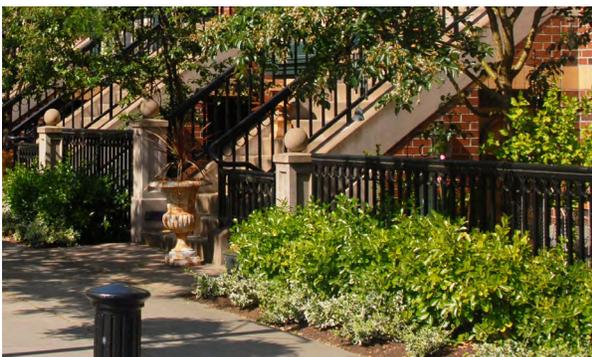
Shade structures help to make the sidewalk more comfortable for pedestrians.

6.2.6 LIGHTING

- A. **Fixtures.** In LASP-CG, pedestrian-scale lighting, such as sconces and goose-neck fixtures, shall be located on the building frontage at least every 25 feet along Lincoln Avenue.
1. Fixtures shall be placed between 8 and 15 feet above sidewalk elevation, and shall not project more than 30 inches from the façade.
 2. Lighting shall be static; flashing, pulsating or other dynamic lighting is not permitted.

6.2.7 WALLS & FENCES

- A. **Walls and Fences.** Walls, fences, and similar structures are permitted within the street setback up to a maximum height of 42 inches.
1. Walls and fences taller than 30 inches shall be at least 50% transparent and shall be set back 24 inches from the sidewalk line, separated by planted area.
 2. In LASP-RM-16, all walls and fences shall be set back 24 inches from the sidewalk line, separated by planted area.
 3. Exceptions to height allowed for guardrails, which may exceed the maximum height to the extent required by the Building Code. The guardrail shall be at least 50% transparent.
 4. Exceptions to setback allowed for outdoor dining, which may be enclosed by a wall, fence, or similar structure up to 42 inches in height located at the sidewalk line.
- B. **Stoops and Patios.** Walls along the side of a stoop, patio or entry to a residential dwelling unit shall be set back at least 24 inches from the sidewalk line, separated by planted area.



Appropriate residential fence height and placement

6.2.8 BALCONIES & ROOF DECKS

- A. **Balconies.** Balconies may project up to 4 feet into a street setback but not past the sidewalk line, and no closer than 6 feet to an interior property line. Balconies shall not project from a building façade within 50 feet of an RS zoning district.
- B. **Roof Decks.** Roof decks shall be set back 5 feet from the building edge on all sides, and shall not be located within 50 feet of an RS zoning district. The sum of all roof decks shall cover no more than one-third of the roof area.

6.3 Open Space

These standards are intended to:

- » Provide a variety of open space types for gathering, recreation and respite that contribute to enhanced livability within an urban setting;
- » Give residents access to natural light and fresh air in and around their living spaces;
- » Improve building design and site planning through the integration of open space throughout the development; and
- » Correlate open space requirements with number of residents and size of buildings.



Private Open Space (Patio and Balconies)

IMPORTANCE OF OPEN SPACE

A variety of high quality, usable and accessible open space contributes to an active public realm and successful building design. A combination of **Private**, **Common**, and **Public Open Space** serves a range of purposes, including spaces for relaxation and community gathering for residents, employees, and visitors within an urban setting. Open spaces either on the ground floor or on upper level stories, correlated to the building use and size, can also help to break up building massing creating effective site and building design.

TYPES OF OPEN SPACE

- » **Private.** Private Open Spaces (e.g. patios and balconies) are not freely open to the public and are only accessible from individual units.
- » **Common.** Common Open Spaces are shared among tenants of a building and often take the form of courtyards and pool areas. Common Open Space can also include shared indoor spaces, such as lounges, community kitchens, and gyms.
- » **Public.** Public Open Spaces, e.g. plazas, pocket parks, and paseos, are freely available to the public to use and typically include amenities such as seating, landscaping, fountains, and public art.

6.3.1 MINIMUM AREA

- A. **Private and Common Open Space.** Projects shall provide the minimum area of Open Space based on use and size per Table 6.3-1. Areas used regularly for parking, loading or storage do not count towards minimum Open Space requirements.
 - 1. **Residential.** Projects with dwelling units shall provide the minimum area of Open Space per Table 6.3-1 as Private and/or Common Open Space.
 - 2. **Non-residential.** Projects with more than 40,000 square feet of non-residential uses shall provide at least 5% of the gross non-residential floor area as Common Open Space, which may be open to the public; see Section 6.3.3.
 - 3. **Mixed-use.** Projects shall comply with requirements applicable to each type of use.
- B. **Public Open Space.** In LASP-CG, projects with more than 15,000 square feet of building floor area shall provide 5% of building floor area as Public Open Space. In LASP-CF, projects with more than 80,000 square feet of building floor area shall provide a percentage of building floor area as Public Open Space, as set in Table 6.3-2.



Common Open Space (Courtyard)

Table 6.3-1: Residential Open Space by Unit Type

Number of Bedrooms	0	1	2	3+
Per Unit, sq ft	200	225	250	275

6.3.2 PRIVATE OPEN SPACE

- A. **Dimensions.** A minimum area of 40 square feet with a dimension of at least 5 feet in each direction is required for Private Open Space.
- B. **Distribution.** No more than 40% of the required residential Open Space shall be private to individual tenants.
 - 1. All Private Open Space shall be outdoors.
 - 2. Private Open Space may be located within a required setback.



Private Open Space (Patio)

6.3.3 COMMON OPEN SPACE

- A. **Dimensions.** A minimum area of 400 square feet with a dimension of at least 15 feet in each direction is required for Common Open Space.
- B. **Distribution.** At least 60% of the required residential Open Space shall be common or shared among tenants.
1. At least 70% of Common Open Space shall be outdoors, and at least 80% of outdoor Common Open Space shall be open to the sky.
 2. No more than 50% of Common Open Space may be on a building's rooftop, defined as any roof area on the highest two floors of the structure.
 3. No more than 30% of Common Open Space may be indoors. Indoor Common Open Space shall not include spaces used primarily for circulation.
- C. **Landscaping.** A minimum of 25% of Common Open Space shall be planted area at least 30 inches in each direction. Plant materials shall be selected in compliance with PMC 17.44.050.
- D. **Trees.** A minimum of one 24-inch box tree per project or for every 500 square feet of outdoor Common Open Space, whichever is greater, shall be planted within Common Open Space. For projects with 2 or more trees, at least 50% of trees planted shall be shade trees.
- E. **Hardscape.** A maximum of 25% of Common Open Space may be paved in standard concrete. Remaining areas shall use one of the following enhanced paving techniques: brick, natural stone, unit concrete pavers, textured and colored concrete, concrete with exposed or special aggregate. Alternative paving may be allowed per review authority approval.
- F. **Water Features.** A maximum of 5% of Common Open Space shall be fountains, reflecting pools, or other decorative water features. Swimming pools are not considered water features for the purposes of this standard.
- G. **Access.** Common Open Spaces may be accessible to the public if desired; see 6.3.4.B.

CREATING COMMUNITY GATHERING SPACES

Common open spaces provide areas for gathering, recreation, and respite within a development.



Communal picnic area with moveable seating options



Enhanced paving, seating, and landscaping

6.3.4 PUBLIC OPEN SPACE

Minimum area requirements set in 6.3.1.B.

- A. **Dimensions.** A minimum area of 400 square feet with a dimension of at least 15 feet in each direction is required for Public Open Space.
- B. **Access.** At least 50% of Public Open Space shall be accessible to the general public and shall not be restricted to patrons of a particular business.
- C. **Signage.** Public Open Space shall have signage visible from the adjacent sidewalk identifying the space as a publicly-accessible amenity and listing accessible hours.
- D. **Hours.** At a minimum, Public Open Space shall be open to the general public from 8am to 8pm.
- E. **Elevation.** A minimum of 3,000 square feet of Public Open Space shall be at sidewalk elevation. If less square footage is required, then all required Public Open Space shall be at sidewalk elevation.
- F. **Hardscape.** A maximum of 25% of Public Open Space shall be paved in standard concrete. Remaining areas shall use one of the following enhanced paving techniques: brick, natural stone, unit concrete pavers, textured and colored concrete, concrete with exposed or special aggregate. Alternative paving may be allowed per review authority approval.

Table 6.3-2: LASP-CF Public Open Space by Project Size

80,000-119,999 ft ²	120,000+ ft ²
2%	3%

- G. **Seating.** Seating accessible to the general public shall be provided at a minimum of 1 seat per 250 square feet of required space. Fractions shall be rounded down to the nearest whole number.
- H. **Landscape.** A minimum of 10% of Public Open Space shall be planted area at least 30 inches in length, width, and depth. Plant materials shall be selected in compliance with PMCI7.44.
- I. **Trees.** A minimum of one 24-inch box tree per project or per each 750 of Public Open Space, whichever is greater, shall be planted. For projects with 2 or more trees, a minimum 50% of trees planted shall be shade trees.
- J. **Common Open Space Credit.** Public Open Space may count towards up to 30% of the Common Open Space requirement at a 1:1 ratio.



Public Open Space (Plaza)

6.4 Parking

These standards are intended to:

- » Reduce the visual impacts of parking;
- » Regulate appropriate parking supply and location in a manner that prioritizes pedestrian access and multi-modal activity;
- » Encourage change of use and adaptive reuse of existing buildings through parking reductions and exemptions;
- » Promote a more efficient use of space through shared parking among multiple uses; and
- » Increase design standards for parking structures by ensuring habitable floor area and screening between parking and street frontage.

6.4.1 MINIMUM PARKING

- A. **Number of Spaces.** Projects shall provide off-street automobile parking spaces per Table 6.4-1 based on general use classifications.
 - 1. Bicycle parking shall be required per PMC 17.46.320.
- B. **Shared Parking.** Parking may be shared among multiple uses per PMC 17.46.050.

IMPORTANCE OF PARKING

Vehicle parking access, location and supply influences the street environment, multi-modal travel and overall development. Managing the location of vehicular access, such as entries and driveways, can help to promote continuous sidewalk activity and safer travel across modes. Similarly, minimizing surface parking lot size and locations (such as placing lots behind buildings or a landscaped open space), supports the success of street-fronting activity, such as pedestrian travel and commercial frontages. The number of required parking spots is another defining factor that shapes urban travel and development. By establishing an appropriate number of parking spots by land use and size of development, residential and commercial activity can be supported while also attracting a variety of new development. Through tailored standards as well as parking reductions and exemptions for certain uses and conditions, space efficiency and cost savings are promoted.

Table 6.4-1: Minimum Parking by Land Use

Use Classification ¹	Number of Spaces	Exceptions
Residential	<ul style="list-style-type: none"> ≤1-bed: 1 per unit ≥2-bed: 1.5 per unit Guest: 1 per 10 units 	Guest parking may be shared with commercial parking in mixed-use projects
Recreation, Education & Public Assembly	PMC 17.46.040	
Office, Professional & Business Support	<ul style="list-style-type: none"> 2 per 1,000 sq ft in LASP-CG; 3 per 1,000 sq ft elsewhere 	No parking required for: <ul style="list-style-type: none"> • First 5,000 sq ft of a project • First 500 sq ft of outdoor dining (per tenant)
Retail Sales (including Restaurants)		
Services		
Industry, Manufacturing & Processing	PMC 17.46.040	
Transportation, Communications & Utility	PMC 17.46.040	
Other Exceptions		
No new parking required for: <ul style="list-style-type: none"> • Projects within designated historic resources (excluding additions) • Changes of use in structures built prior to 1970 		
¹ Use classifications correspond to general use categories in PMC 17.46.040. The number of spaces listed above shall apply to all uses listed under these general categories, with the exception of specific uses where the parking requirement is lower per PMC 17.46.040.		

- C. **Unbundled Parking.** For any building with new residential units, automobile parking spaces shall be leased or sold separately from the rental or purchase fees, such that renters or buyers have the option of renting or buying the residential unit at a lower price than if the parking was included.
 1. For deed-restricted affordable units, one parking space shall be included in the base rent of each unit. The tenant may choose to receive the parking space or receive a rent discount equivalent to half the amount charged for monthly lease of a parking space. Tenants of affordable units shall not sublease their parking spaces.
 2. Renters or buyers have the right of first refusal to parking built for their unit. Any remaining spaces may be leased to other users on a month-to-month basis. New occupants shall have the opportunity to lease or purchase parking built for their unit.

6.4.2 VEHICLE ACCESS

- A. **Driveways.** For Projects with less than 200 feet of primary street frontage, a maximum of one two-way driveway shall be permitted. For Projects with more than 200 feet of primary street frontage, a maximum of two two-lane driveways shall be permitted.
 1. Driveways shall not be not permitted on primary frontages of 200 feet or less where there is access from a secondary street or alley.

6.4.3 LAYOUT & DESIGN

- A. **Surface Parking.** Parking lots shall be set back at least 30 feet from the primary frontage, 10 feet from any secondary frontage, and 5 feet from RS zoning. Parking shall be buffered by habitable floor area or landscaped open space, except for access and driveways.
 1. Landscaping shall include hedges or shrubs at least 3 feet in height at the time of planting that form a continuous visual screen.
- B. **Podium Parking.** No more than one story of above-grade parking is permitted within habitable buildings. Multiple stories of above ground parking are permitted within wrap-style developments or parking structures if the project meets the parking structure standards below.
- C. **Parking Structures.** Structures shall be buffered with habitable floor area between the parking and Lincoln Avenue, except for access and driveways.
 1. Parking structures shall not be permitted in the LASP-MU.
 2. Elevators and stairs shall be located adjacent sidewalks or public spaces.
 3. Parking areas shall be screened from the public realm using heavy-gauge metal, precast concrete panels, laminated glass, green walls, photovoltaic panels or other material per review authority approval.
- D. **Underground Parking.** Fully subterranean parking shall be set back at least 5 feet from RS zoning. Otherwise, it may extend up to the property line.

DESIGN GUIDELINES



Entrances to structured parking should be integrated into façade design and shall screen upper portions of the entrance not required for vehicle clearance.



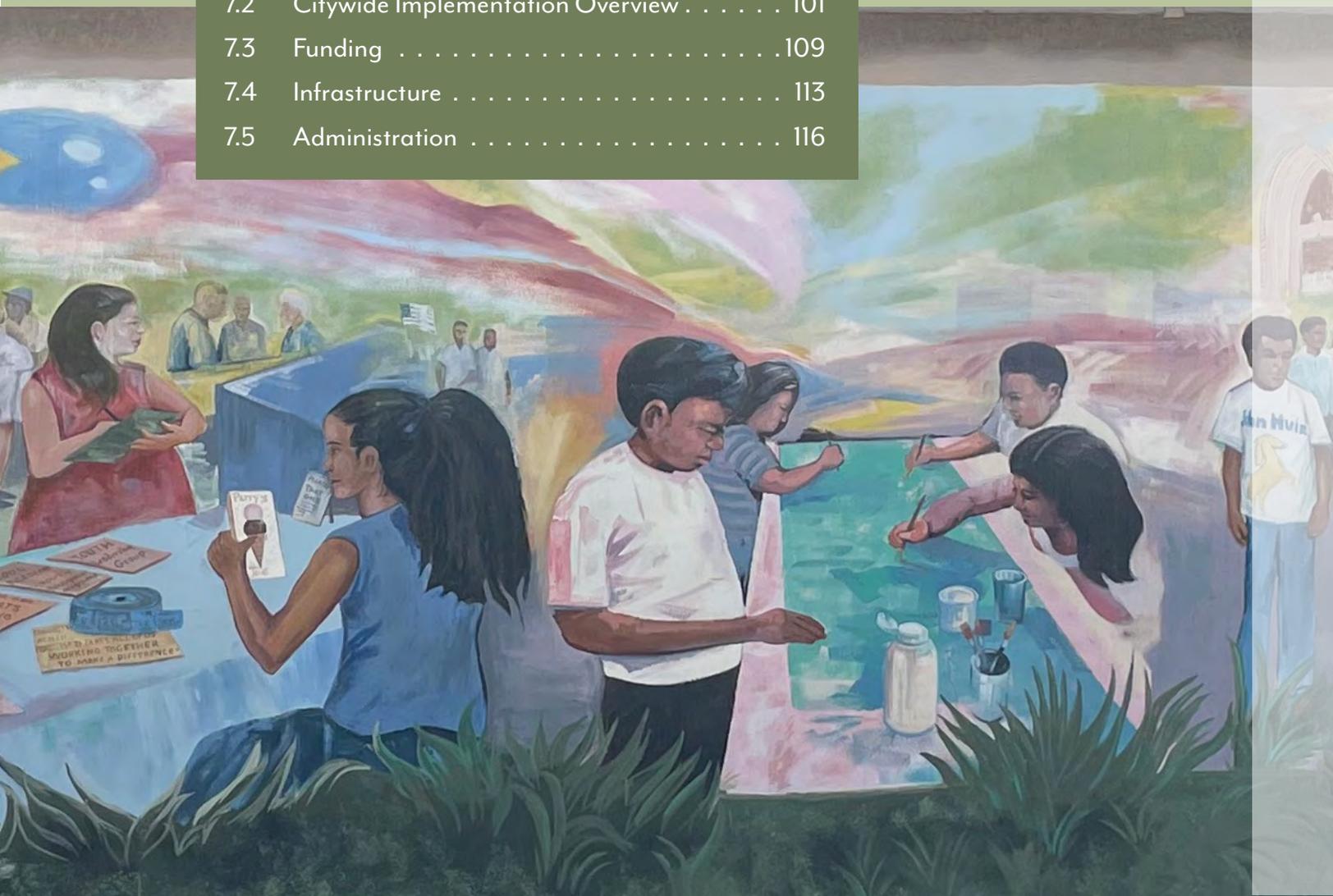
Example of parking entrance design without screening or façade integration.

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Ch. 7

Implementation & Administration

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John Muir High School

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Implementation & Administration

CHAPTER OVERVIEW

The Lincoln Avenue Specific Plan will be primarily implemented through the adoption of the Plan's new Land Use, Public Realm, and Development regulations, which align private sector investment with the Plan's vision, goals, and policies. In addition, public sector improvements and programs funded through the City and outside sources, can further implement the Plan, particularly where redevelopment is less likely to occur.

This chapter includes targeted implementation actions intended to help guide and prioritize the implementation of the Plan. It also provides an overview of existing services, plans, and programs, all of which can be leveraged to help implement the Plan. A summary of funding sources is also included to guide the City in understanding and selecting available funding sources to implement the improvements and programs identified in this Specific Plan. This chapter concludes with Specific Plan administration.

This chapter is organized into the following sections:

- » **7.1. Implementation Actions**
- » **7.2. Citywide Implementation Overview**
- » **7.3. Funding**
- » **7.4. Infrastructure**
- » **7.5 Administration**



7.1 Implementation Actions

Specific Plans are used by various City departments when reviewing projects, seeking funding, and to understand the vision, goals, and policies of specified geographic areas to guide improvements and programming. Implementing specific plans requires collaboration across City departments and coordination with existing citywide implementation programs, plans, and efforts.

The following implementation actions are intended to guide the City in implementing the Lincoln Avenue Specific Plan over time with generalized timeframes as follows:

- » **Ongoing:** Periodic or on a continuing basis
- » **Immediate:** Upon adoption of the Specific Plan
- » **Near-Term:** 0-5 years
- » **Medium-Term:** 5-10 years

As changes in City priorities, economic conditions, and market trends occur over time, the City may need to revisit and reprioritize the implementation actions. Table 7.1-1 and the following sections outline implementation actions for the Lincoln Avenue Specific Plan, including description, timeframe, and responsible parties for each action. Information included for each action is intended to help guide the City in taking next steps, which will include additional planning, coordination, community input, and public processes.

Table 7.1-1: Lincoln Avenue Specific Plan Implementation Actions

Action	Description	Timeframe	Responsible Parties
Amendments (A)			
A-1: General Plan Map and Text Amendment	Amend General Plan Land Use Diagram to adjust Lincoln Avenue Specific Plan boundary and update land use designations per Chapter 4 (Land Use).	Immediate	P&CDD
A-2: Zoning Code Map and Text Amendment	Amend the Zoning Code to replace existing development standards with the development standards provided in Chapters 4-6 of this Specific Plan. Amend Zoning Map to replace zoning district designations indicated on the Zoning Map with the new Lincoln Avenue Specific Plan zoning districts.	Immediate	P&CDD
A-3: Specific Plan Amendment	Amend the Lincoln Avenue Specific Plan including new goals and policies, as well as land use and development standards.	Immediate	P&CDD

Action	Description	Timeframe	Responsible Parties
Infrastructure, Mobility, and Sustainability (IMS)			
IMS-1: Master Street Tree Plan	Review street tree palette from the Master Street Tree Plan to consider tree designations that better address Specific Plan and Climate Action Plan objectives for climate resilience, shading, urban cooling, and carbon sequestration and which are complementary to adjacent development and uses.	Near-Term	PWD
IMS-2: Complete Streets Program Improvements	Work with DOT to identify opportunities for pedestrian signalized crossings to improve east-west pedestrian safety and mobility across Lincoln Avenue and bulb-outs with sustainable elements, like bioswales, as part of DOT's Complete Streets Program.	Near-Term	DOT PWD P&CDD
IMS-3: Streetscape Program	Explore opportunities to develop a Streetscape Program for Lincoln Avenue, including improvements that address pedestrian amenities, such as seating, in alignment with DOT's Pedestrian Plan.	Medium-Term	DOT PWD P&CDD
IMS-4: Bicycle Transportation Action Plan Facilities	Consider Lincoln Avenue as a potential corridor for a future bicycle facility to provide north-south connections to greenway along Villa Street and buffered bicycle lane along Washington Boulevard proposed by DOT's BTAP.	Medium-Term	DOT
IMS-5: Parking Management Program	Explore a parking management program for the Lincoln Main Street zoning district that reduces or eliminates parking requirements for individual properties and identifies other parking supply opportunities.	Medium-Term	DOT P&CDD
IMS-6: Planning Study for I-210 Freeway Underpass	Conduct a planning study to improve the I-210 Freeway underpass for pedestrians and the connection between north and south Lincoln Avenue, including investigating feasibility of widening the sidewalk into northbound parking lane as part of DOT's Complete Streets Program. If determined to be feasible, consider developing conceptual design and cost estimates to support the next step of identifying potential funding sources for implementation.	Medium-Term	PWD DOT P&CDD

Action	Description	Timeframe	Responsible Parties
Community Identity, Programming and Public Art (PA)			
PA-1: Citywide Rotating Public Art Program	Consider placing temporary public art along Lincoln Avenue as part of the Citywide Rotating Public Art Program.	Ongoing, Medium-Term	P&CDD
PA-2: Temporary Art Installations in Empty Storefronts	Connect building owners with arts organizations to develop new temporary art installations in empty storefronts along Lincoln Avenue.	Medium-Term	P&CDD
PA-3: Pedestrian-Oriented Art in Public Realm	Explore opportunities for pedestrian-oriented art on commercial and mixed-use portions of Lincoln Avenue through artist-designed crosswalks, utility boxes, and murals, as well as enhancements to blank facades, light poles, medians, and parking strips.	Medium-Term	P&CDD
PA-4: Public Artwork in I-210 Freeway Underpass	Explore opportunities to integrate public art and gateway concepts as part of I-210 Freeway underpass planning study (see IMS-2) in coordination with Caltrans.	Medium-Term	PWD P&CDD
PA-5: Muir High School Front Lawn Programming	Explore opportunities to utilize the John Muir High School front lawn for outdoor community programming.	Medium-Term	EDD PR&CS PWD PUSD P&CDD
PA-6: Business Association Formation	Consider formation of a business and/or property-owner association to strengthen opportunities for placemaking and community identity, in addition to marketing along Lincoln Avenue.	Medium-Term	EDD NW

RESPONSIBLE AGENCIES:

- » **P&CDD:** Planning & Community Development Department
- » **DOT:** Department of Transportation
- » **PWD:** Public Works Department
- » **PUSD:** Pasadena Unified School District
- » **NW:** Northwest Programs Office
- » **EDD:** Economic Development Department
- » **PR&CS:** Parks, Recreation and Community Services

7.2 Citywide Implementation Overview

The City of Pasadena currently provides a wide variety of services and programs either directly or through partnerships with local non-profits, many of which can support the implementation of specific plans. Services and programs relevant to the Lincoln Avenue Specific Plan that are implemented through citywide efforts are listed below. Learn more about the organization of the City of Pasadena, including how various city departments are structured and connected in Figure 7.2-1.

1. General Fund

WHO: Mayor & City Council

WHAT: Primary fund of the City that is used to account for all general revenues of the City not specifically levied or collected for other City funds and for expenditures related to the rendering of general services by the City. Operating and capital budgets are created using guiding principles to determine budget priorities.

WHEN: Every year the City Council adopts an Operating Budget allocating resources to fund vital public services and programs for everyone who lives, works and plays in the City of Pasadena.

LEARN MORE HERE:

<https://www.cityofpasadena.net/finance/general-fund/>

2. Capital Improvement Program

WHO: Department of Public Works

WHAT: The City appropriates annual capital funds by department and project category through the Capital Improvement Program (CIP). The CIP budget consists of projects aimed at improving the city's public infrastructure such as streets, transportation issues, street lights, traffic signals, parks, public buildings, sewer and storm drains, the Rose Bowl, the Pasadena Center, technology and water and power projects. Projects can be short, medium or long-term.

WHEN: The CIP Budget is submitted annually to the City Council as a separate budget document in order to provide more detailed descriptions of City Capital Improvement Projects scheduled to take place over the course of the 5-year lifetime of the document.

LEARN MORE HERE:

<https://www.cityofpasadena.net/public-works/engineering-and-construction/capital-improvement-program/>

3. Master Street Tree Plan

WHO: Urban Forestry Program, Department of Public Works

WHAT: Serves as the guiding document that designates the official tree species to be planted on a block-by-block basis throughout the City. The goal of the Master Street Tree Plan (MSTP) is to promote a uniform urban design on a neighborhood scale, while also promoting species diversity city-wide. With the development and expansion of the City, and with changes in arboricultural practices, the MSTP has been revised and amended accordingly.

WHEN: Periodically



LEARN MORE HERE:

<https://www.cityofpasadena.net/public-works/urban-forestry/#master-street-tree-plan>

4. Tree Protection Ordinance

WHO: Urban Forestry Program, Department of Public Works

WHAT: The City Trees and Tree Protection Ordinance was adopted as Chapter 8.52 of the City's Municipal Code in 2002. The Tree Protection Ordinance includes measures to protect four categories of trees including (1) public trees, (2) landmark trees, (3) native trees, and (4) specimen trees in certain areas of the City. The process for designating landmark trees is included in the ordinance, in addition to requirements for removal and pruning of protected trees. The ordinance also includes Tree Protection Guidelines that seek to avoid negative impacts to protected trees that may occur during construction. If provisions are violated, the ordinance outlines penalties and administrative proceedings.

WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/public-works/urban-forestry/#pasadena-tree-ordinance>

5. Complete Streets Program

WHO: Department of Transportation

WHAT: Implements Assembly Bill 1358, known as the Complete Streets Act, enacted in 2008, to reduce greenhouse gas emissions, make the most efficient use of urban land and transportation infrastructure, and improve public health through shifting short trips from automobiles to biking, walking and use of public transit. The Mobility Element (2015) of the City's General Plan guides the Department of Transportation (DOT) through goals and objectives that address complete streets. DOT implements Complete Streets through the Pasadena Street Design Guide (2017), the Bicycle Transportation Action Plan (2015), and Pasadena Pedestrian Plan (2006).



WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/transportation/>

6. Water Conservation, Recycling, Stormwater Management

WHO: Department of Water and Power

WHAT: Pasadena Water and Power (PWP) is a community enterprise that provides electricity and water to the Pasadena community. The PWP General Manager reports to the City Manager and is governed by the City Council. The Urban Water Management Plan (2015) provides an analysis of long-term water supply and demand planning for PWP, including system analysis, reliability assessment, water-use targets, water shortage contingency planning, demand management and climate change impact.

WHEN: Prepared every five years in compliance with the Urban Water Management Planning Act (California Water Code Sections 10610 through 10656).

LEARN MORE HERE:

<https://www5.cityofpasadena.net/water-and-power/uwmp/>

7. Energy and Energy Efficiency

WHO: Department of Water and Power

WHAT: Through the Power Integrated Resource Plan (IRP), Pasadena Water and Power (PWP) sets steps for upholding local, state and federal mandates and internal power supply goals, including having a balanced and sustainable mix of sources towards a green portfolio in the future. Renewal energy sources include solar, wind, geothermal, landfill gas, and hydropower. As part of energy efficiency and sustainability, PWP also has several programs and initiatives including electric vehicles, solar, green power, greywater, drought-tolerant landscapes and enhancing Pasadena's watershed.

WHEN: Ongoing

LEARN MORE HERE:

<https://ww5.cityofpasadena.net/water-and-power/power/>

<https://ww5.cityofpasadena.net/water-and-power/sustainability/>

8. Climate Action Plan

WHO: Planning & Community Development Department, Department of Public Works, Department of Transportation and Department of Water and Power

WHAT: The Climate Action Plan (2018) is a strategic framework for measuring, planning, and reducing the City's share of greenhouse gas emissions by more than half by the year 2035. The CAP includes measures that are regulatory, incentive-based, or voluntary and are implemented by multiple City departments, including PWP, PW, DOT, and Planning. An annual report is prepared to track the CAP's progress and its implementation.



WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/planning/planning-division/community-planning/climate-action-plan/>

9. Development Impact Fees

WHO: Department of Public Works

WHAT: The City charges development impact fees on new development to offset the cost of public facilities related to the development, in turn helping to fund implementation actions, such as improvement projects. While impact fees associated with new development are updated periodically, current fees include residential impact fees to fund affordable housing, traffic reduction and transportation improvement fees, sewer facility charges fees, and feeds for funding public art. Pasadena’s development impact fees are calculated based on the number of bedrooms

or gross built area (for the residential and transportation fees) for estimated project value (for public art fees). Impact fees are directed to the General Fund, which funds initiatives in the associated fee categories.

WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/planning/permit-center/fee-schedules/>

10. Parks, Recreation and Community Services

WHO: Parks, Recreation and Community Services

WHAT: Provides the City with recreational and human service programs focused on preserving and improving the physical, social, and economic health of Pasadena neighborhoods. The parks and recreation portion of the Department is guided by the City’s General Plan Green Space, Parks and Recreation Element and Master Plan (2007), which work together to assess existing facilities and programs, identify additional needed parking facilities or recreation programs, and recommend best methods to meet needs. New parks projects, including planning and design studies for new parks and the construction of green spaces, facilities, and community centers are funded in part through the CIP, in addition to other funding sources. Community services are also provided through the Department, including assistance with landlord/tenant issues and other housing rights topics.



WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/parks-and-rec/>

11. Public Art Program

WHO: Arts and Cultural Affairs Division

What: The Public Art Program focuses on building a publicly available collection of contemporary art. The Cultural Nexus Plan (2004) and the Public Art Master Plan (2014) guide the Public Art Program through established cultural policies and a vision for new public art development in Pasadena, supported by goals and objectives with strategies for implementation. The City's Public Art Program includes Public Art Requirements that focus on two areas: new private development and City construction (CIP) projects. The Requirements may be satisfied by the creation of a site-specific public art or by payment in-lieu of artwork. In addition, the Public Art Program includes a Rotating Public Art Exhibition Program that complements the permanent artworks commissioned by the City's Public Art Requirements by temporarily installing contemporary sculptures in each of Pasadena's seven Council Districts.



WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/planning/permit-center/fee-schedules/>

12. Northwest Programs Office

WHO: Northwest Programs Office

WHAT: Serves as a liaison between the community and the City Manager's Office to identify and address the needs of Northwest Pasadena, maintain a healthy business environment, and facilitate development projects through the entitlement process in coordination with other departments. One of several commissions/committees included in the CIP budget process for review and support, the Northwest Commission reviews all projects located in Northwest Pasadena.

WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/northwest/>

13. Economic Development Division

WHO: Economic Development Division

WHAT: Responsible for encouraging business investment opportunities and supporting business retention and attraction activities in Pasadena. As part of the City Manager's Office, the Economic Development Division provides resources to help businesses, including assistance locating a site for a new business, training for new and small businesses, intel on consumers, and networking. Business Improvement Districts (BIDs) can be created through the Economic Development Division to establish defined areas within which businesses are typically required to pay an additional tax to help fund improvements and projects, such as sidewalk cleaning, trash pick-up, and programming.

WHEN: Ongoing

LEARN MORE HERE:

<https://www.cityofpasadena.net/economicdevelopment/>

14. Pasadena Unified School District

WHO: Pasadena Unified School District

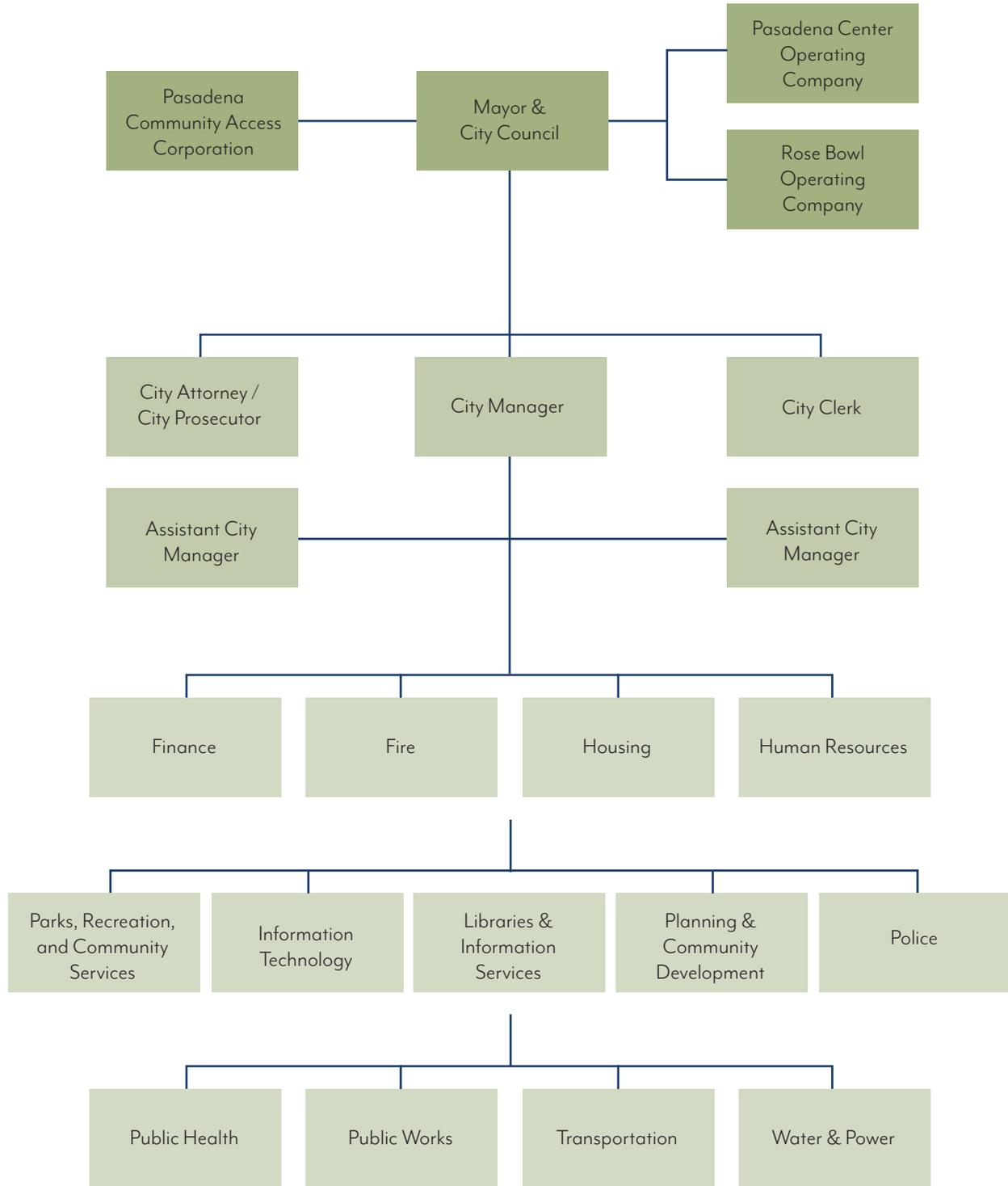
WHAT: Provides education for more than 17,000 students in transitional kindergarten-12th grade across a 76-square mile area. PUSD's school campuses include buildings and facilities that provide classrooms, libraries, laboratories, playing fields, open space and others. Through an existing partnership with PUSD, the City of Pasadena utilizes school campus buildings and facilities for city-sponsored community events, meetings, and programs. John Muir High School is located directly adjacent to the Lincoln Avenue Specific Plan area, between Idaho Street and Wyoming Street along Lincoln Avenue.

WHEN: Ongoing

LEARN MORE HERE:

<https://www.pusd.us/>

Figure 7.2-1: Pasadena City Organization Chart



7.3 Funding

This section summarizes a variety of potential funding sources that may help the City and community to implement the actions outlined in Table 7.1-1 and support the Specific Plan vision, goals, and policies. While some implementation actions can be implemented incrementally, others occur with development projects, and some others will require capital funding from a variety of sources. It is beneficial to have outside funding to expedite implementation of the Lincoln Avenue Specific Plan improvements as City revenues and fees summarized in the previous section are limited.

Typical outside sources of funding for pedestrian enhancements, streetscape improvements, public art, and affordable housing applicable to Lincoln Avenue Specific Plan are summarized in Table 7.3-1 and described in further detail in the following section. This list of sources is not exhaustive but is intended to provide a starting point for developing a funding strategy for Lincoln Avenue improvements and programs. Funding programs are subject to change; the programs listed in this chapter are relevant as of the time of Plan adoption. As noted in the following section, grant applications for projects in the Lincoln Avenue Specific Plan may be more successful if “bundled” with projects in other parts of the city.

Funding Source Category	Funding Source	Improvement Category			
		Pedestrian Enhancements	Streetscape	Public Art	Affordable Housing
Regional, State, and Federal Programs	Affordable Housing and Sustainable Communities Program	✓	✓		✓
	Sustainable Communities Competitive Grants	✓	✓		
	Active Transportation Program (ATP)	✓	✓		
	Urban Greening Program	✓	✓		
	Environmental Enhancement and Mitigation (EEM) Program	✓	✓		
	California Infrastructure and Economic Development Bank (I-Bank)	✓	✓		✓
	Community Development Block Grant (CDBG) Program	✓	✓		✓
	Metropolitan Transportation Authority (Metro) Call for Projects	✓	✓		✓
	New Markets Tax Credit (NMTC)	✓	✓	✓	✓

7.3.1 REGIONAL, STATE, AND FEDERAL PROGRAMS

AFFORDABLE HOUSING AND SUSTAINABLE COMMUNITIES PROGRAM

The Affordable Housing and Sustainable Communities Program (AHSC) funds land use, housing, transportation, and land preservation projects to support infill and compact development that reduce greenhouse gas emissions. Administered through the California Department of Housing and Community Development (HCD), funding for the AHSC Program is provided from the Greenhouse Gas Reduction Fund, an account established to receive Cap-and-Trade auction proceeds. 50 percent of the available funds are set aside for Affordable Housing Developments, and 50 percent of the available funds are set aside for projects benefitting Disadvantaged Communities. The majority of Lincoln Avenue Specific Plan is currently designated as a Disadvantaged Community (DAC) per CalEnviroScreen (version 3.0) which enables the Plan Area to benefit from the set aside funds for DACs, focusing on increasing accessibility of affordable housing, employment centers, and key destinations via low-carbon transportation, such as transit, bicycling, or walking.

A contiguous area with at least one transit station/stop must be established for eligibility, including a flexible transit service route which applies to Lincoln Avenue Specific Plan.

SUSTAINABLE COMMUNITIES COMPETITIVE GRANTS

The Sustainable Communities Competitive Grants fund transportation planning activities such as planning for active transportation, safe routes to schools, transit services, vision zero, complete streets, freight corridors, social equity, integrated land use and transportation planning. Grants are available on an annual basis and through a competitive application process managed by Caltrans.

ACTIVE TRANSPORTATION PROGRAM (ATP)

The Active Transportation Program (ATP) funds projects that encourage increased use of active modes of transportation to increase the proportion of trips accomplished by biking and walking, increase safety and mobility for non-motorized users, advance the active transportation efforts of regional agencies to achieve Greenhouse Gas (GHG) reduction goals, enhance public health, ensure that disadvantaged communities fully share in the benefits of the program, and provide a broad spectrum of projects to benefit many types of active transportation users. ATP consolidates existing federal and state transportation programs, including the Transportation Alternatives Program (TAP), Bicycle Transportation Account (BTA), and State Safe Routes to School (SRTS), into a single program.

The students at John Muir High School, as well as other nearby schools, could benefit from SRTS funds to improve active transportation and access to school campuses.

URBAN GREENING PROGRAM

The Urban Greening Program funds a variety of improvement projects, including urban heat island mitigation and energy conservation efforts, green streets and alleyways, non-motorized urban trails that provide safe routes for travel between residences, workplaces, commercial centers and schools, and others. The program is part of California Climate Investments, a statewide initiative that puts billions of Cap-and-Trade dollars to work reducing greenhouse gas emissions, strengthening the economy, and improving public health and the environment — particularly in disadvantaged communities.

If several improvement projects were bundled together, there is potential for Urban Greening program to fund some Lincoln Avenue SPA improvements if the project can demonstrate improved safety or quality of life.

ENVIRONMENTAL ENHANCEMENT AND MITIGATION (EEM) PROGRAM

The Environmental Enhancement and Mitigation (EEM) Program was established by the Legislature in 1989 to fund environmental enhancement and mitigation projects directly or indirectly related to transportation projects. EEM Program projects must fall within one of three categories: highway landscape and urban forestry; resource lands; or roadside recreation. Projects funded under this program must provide environmental enhancement and mitigation over and above that otherwise called for under the California Environmental Quality Act (CEQA).

CALIFORNIA INFRASTRUCTURE AND ECONOMIC DEVELOPMENT BANK (I-BANK)

The California Infrastructure and Economic Development Bank (I-Bank) finances public infrastructure and private development through issuing tax-exempt and taxable revenue bonds, providing financing to public agencies, providing credit enhancements, acquiring or leasing facilities, and leveraging State and federal funds.

The Infrastructure State Revolving Fund (ISRF) Program provides financing to public agencies and non-profit corporations for 18 categories of infrastructure and economic development projects. ISRF Program funding is available in amounts ranging from \$50,000 to \$25,000,000, with loan terms of up to 30 years.

CAP-AND-TRADE PROGRAMS

The State administers a growing number of grant and loan programs, collectively known as the California Climate Investments Program (CCIP), that provide funding for projects and programs that reduce greenhouse gases (GHGs) and provide health, mobility, economic, and other co-benefits to communities throughout the state. Most Cap-and-Trade programs target a substantial portion of their funding to Disadvantaged Communities (DAC) classified in accordance with the CalEnviroScreen tool. The majority of the Lincoln Avenue Specific Plan area currently qualifies as a DAC per CalEnviroScreen (version 3.0), defined as the top 25% scoring areas based on ranking each of the State's 8,000 census tracts using national and state data sources on 20 indicators of pollution, environmental quality, and socioeconomic and public health conditions. This designation establishes priority and a competitive advantage for projects that would benefit the local community in seeking Cap-and-Trade funding. Cap-and-Trade provides funding in three primary areas:

1. Transportation and Sustainable Communities
2. Clean Energy and Energy Efficiency Funding
3. Natural Resources and Waste Diversion Funding

Under each of these funding areas are numerous programs that have funding available for projects and programs that would either be contained within Lincoln Avenue Specific Area or benefit the SPA and the City as a whole. Programs with high applicability to Lincoln Avenue Specific Plan are summarized in this section.

COMMUNITY DEVELOPMENT BLOCK GRANT (CDBG) PROGRAM

The Community Development Block Grant (CDBG) Program funds revitalization of neighborhoods, expansion of affordable housing and economic opportunities, and/or improvements of community facilities and services, principally to benefit low- and moderate-income persons or neighborhoods. Also eligible are the building of public facilities and improvements, such as streets, sidewalks, sewers, water systems, community and senior citizen centers and recreational facilities.

Operated by the U.S. Department of Housing and Urban Development (HUD), the Community Development Block Grant (CDBG) Program is a federal program that provides grants for economic development, public facilities, and housing rehabilitation. CDBG funds must provide benefits to low- or moderate-income individuals, prevent or eliminate slums or blight, or may be used for other emergency community needs, such as those related to a natural disaster. CDBG funds can be used for development purposes within low- or moderate income census tracts, which applies to the current conditions in Lincoln Specific Plan area, or, if the development or activity is located outside of a low- or moderate-income census tract, funds must provide benefits to low- or moderate-income households.

METROPOLITAN TRANSPORTATION AUTHORITY (METRO) CALL FOR PROJECTS

Every other year, the Metro Call for Projects funds projects across seven modal categories, including pedestrian improvements, which is applicable for Lincoln Avenue Specific Plan. Metro is responsible for allocating discretionary federal, state and local transportation funds to improve all modes of surface transportation. Metro also prepares the Los Angeles County Transportation Improvement Program (TIP). A key component of TIP is the Call for Projects program, a competitive process that distributes discretionary capital transportation funds to regionally significant projects. Local jurisdictions, transit operators, and other public agencies are eligible to submit applications proposing projects for funding.

NEW MARKETS TAX CREDIT (NMTC)

The New Markets Tax Credit (NMTC), a federal tax initiative, could be used to stimulate investment in new development within the Plan area. The NMTC offers tax credits to investors who finance development in low-income communities. These credits are intended to finance minor gaps in project funding and to increase the rate of return for investors. New Markets Tax Credits can fund up to 30 percent of eligible project costs. Projects must create new jobs in the service area and should provide community benefits that would not otherwise be possible solely through private financing. Although residential development is not eligible for the program, commercial space in a mixed-use building or stand-alone commercial projects could be financed in part by the NMTC.

NEW AND FUTURE RESOURCES

As funding opportunities are realized and new funding become available, the City can continue to identify, monitor, and apply for other governmental funding sources that meet the City's and respective agencies' objectives over time. For example, future funding sources might include the Cap-and-Trade Transformative Climate Communities Local Partnership Program, forecasted for 2022, to provide funding to counties, cities, districts, and regional transportation agencies in which voters have approved fees or taxes dedicated solely to transportation improvements or that have imposed fees, including uniform developer fees, dedicated solely to transportation improvements and the HCD Infill Infrastructure Program (draft grant guidelines September/October 2020) to provide grant funding for infrastructure improvements for new infill housing in residential and/or mixed-use projects.

7.4 Infrastructure

The City's existing infrastructure systems and facilities are owned and operated by different departments and other public agencies such as the City's Departments of Public Works and Transportation, Pasadena Water and Power, the Los Angeles County Sanitation Districts and Metropolitan Water District of Southern California. These City departments and other public agencies have processes in place to evaluate existing resources, service area needs, and plan for system upgrades to support growth throughout the City, including the Plan area. The following section identifies how infrastructure facilities for transportation and traffic, wastewater, water supply, solid waste, storm water, and electricity will be provided to meet the anticipated growth.

The 2015 General Plan Update Environmental Impact Report anticipated residential and commercial growth for the entire City through 2035, including the eight Specific Plan areas. Specific information on the analysis and environmental determinations associated with the buildout of the General Plan within the Plan area can be found in the Lincoln Avenue Specific Plan Update Addendum to the General Plan.

7.4.1 TRANSPORTATION

The City has a well-developed transportation network of streets, sidewalks, bicycle facilities, and transit services such as the Metro Light Rail Transit and Bus Service, Pasadena Transit, and Foothill Transit. Three freeways provide regional access to and through the City: the Foothill Freeway (I-210), the Ventura Freeway (SR 134) and the Arroyo Seco Parkway (SR 110). The City's public transportation system includes local bus services, regional bus routes, and light rail. Service is provided by Pasadena Transit, the Metropolitan Transportation Authority (Metro), the Los Angeles Department of Transportation (LADOT) Commuter Express, Foothill Transit and numerous other local transit providers. The City also has Class II bikeways, Class III bike routes, and enhanced bike routes. Additionally, the City has a connected network of pedestrian facilities, designated pedestrian-friendly zones, and upgraded traffic signal technology.

Pasadena DOT helps to implement the Mobility Element of the General Plan through the Bicycle Transportation Action Plan (2015), the Pedestrian Plan (2021), and other projects and programs to enhance the safety and mobility of all modes of transportation. Land use as defined in the General Plan is included in the City's travel demand model which is used to determine the potential impact of new projects and the City monitors traffic operations to identify areas of concerns and address safety and mobility needs.

7.4.2 WASTEWATER SYSTEM

The wastewater system in the Plan area is owned and operated by the City of Pasadena's Department of Public Works and Pasadena Water and Power (PWP), which consists of approximately 328 miles of gravity pipelines and conveys an annual average flow of approximately 14 million gallons per day (MGD).¹ Wastewater from individual services flows into the City's collection system. The City's wastewater collection system conveys untreated wastewater to the Los Angeles County Sanitation District's (LACSD) trunk sewer system for treatment via 92 separate connections.

The Water System and Resources Plan (WSRP) is PWP's 25-year strategy, updated every five years, which includes planning related to the treatment of wastewater, primarily residential. LACSD is responsible for the treatment of wastewater, primarily commercial, from the City. LACSD prepares an annual report that speaks to their mission, core values and major projects for the year. The 2019 annual report notes LACSD is working to turn waste into electricity, compost and other recycling commodities. LACSD works closely with cities to support them in compliance with state and federal regulations for solid waste, green energy, and wastewater.

The City updates the Sewer System Management Plan (SSMP) annually to identify a list of Capital Improvement Program (CIP) projects that take into consideration the age of facilities, construction materials use, current use, capacity, and its condition. The City has undertaken several major projects to ensure sustained reliability of the sanitary collection system. Projects include sewer system improvements and capacity upgrades as well as modernization of pump stations, such as the Busch Garden and Rosemont Sewer Pump Stations.

Developments in the Plan area are subject to wastewater-related requirements and standard conditions of approval, such as payment of development fees and implementation of site-specific Storm Water Pollution Prevention Plan for construction. Development projects are required to comply with all applicable solid waste regulations, including the California Integrated Waste Management Act and the City's Zoning Code Section 17.40.120 (Refuse Storage Facilities).

¹ City of Pasadena Sewer System Management Plan (2018) <https://www.cityofpasadena.net/wp-content/uploads/sites/29/Sewer-System-Management-Plan-SSMP-Final-Report.pdf>

7.4.3 WATER SYSTEM

PWP, a community-owned utility and a not-for-profit public service owned and operated by the City, serves as the water service provider in the Plan area. The PWP water system includes 14 reservoirs with total storage capacity of 110 million gallons, 17 active wells, 19 booster stations, and 1 treatment plant (Monk Hill Water Treatment Plant).² PWP obtains a portion of its water from the local Raymond Basin and purchases imported water from the Metropolitan Water District of Southern California (MWD).

PWP is responsible for evaluating the current and projected needs of customers for potable and non-potable water in the City. The WSRP provides screening of alternatives to meet future demands with necessary infrastructure within operational and financial constraints. PWP's WSRP includes considerations for water quality, greater dependency on local water, groundwater basin stability, reliability of the distribution system, affordability, climate change uncertainties, and legislative and regulatory requirements as well as the treatment of wastewater, primarily residential.

In addition, every five years the City updates its Urban Water Management Plan (UWMP) as required by the California State water code, which includes an analysis of long-term water supply and demand planning for PWP. The 2015 UWMP update included the population projections and land use changes based on the most recent General Plan Update and identified that supplies will exceed demands under all hydrologic scenarios with implementation of additional supplies, such as recycled water and potable reuse, as well as with conservation measures. The 2020 UWMP is currently in development by the City.

7.4.4 SOLID WASTE SYSTEM

Department of Public Works collects solid waste from residences in Pasadena and competes with private haulers for commercial collection. Refuse hauling companies providing commercial solid waste collection are listed on the Department of Public Work Franchise List. Solid waste is disposed of at the following facilities: Calabasas Sanitary Landfill, Scholl Canyon Landfill, Puente Hills Material Recovery Facility, Southeast Resource Recovery Facility, Commerce Refuse-to-Energy Facility, Olinda Alpha Sanitary Landfill, and Frank Bowerman Landfill. All landfills are required to comply with numerous landfill regulations from federal, state, and local regulatory agencies and are subject to regular inspections from CalRecycle and the local enforcement agency, the California Regional Water Quality Control Board, and the South Coast Air Quality Management District.

² City of Pasadena – PWP 'Where our Water Comes From' Webpage <https://ww5.cityofpasadena.net/water-and-power/water/>

The Department of Public Works (DPW) Operations Sections oversee waste management in the City. The DPW is responsible for the solid waste collection and disposal of all residential properties within the City and private haulers compete for commercial collection services in the City in conformance with the City's Municipal Code Chapter 8.61. The Zero Waste Pasadena 2040 (Zero Waste Plan) is DPW's 26-year strategic plan, to be reviewed and updated every three years, that seeks to reduce waste at the source and maximize diversion from landfills with the overall goal of striving for zero waste in the year 2040. The Zero Waste Plan identifies diversion potential, greenhouse gas reduction potential, and materials management.

Developments within the Plan area would continue to be accommodated by existing solid waste service providers and facilities. Future development projects would be subject to the California Green Building Code and solid waste reduction strategies under General Plan policies that continue to encourage the reduction of solid waste through sustainable building practices. Additionally, the City seeks to reduce its solid waste and landfill greenhouse gas emissions in accordance with their Climate Action Plan (CAP) that establishes a goal of reaching an 87% diversion rate by 2035. CAP implementation actions include the Zero Waste Plan, reporting annually on zero waste progress and optimizing waste diversion.

7.4.5 STORMWATER SYSTEM

The City provides storm drainage collection in the Plan area and is responsible for operation and maintenance of the collection system. The system includes open channels, closed conduits, catch basins, laterals, manholes, and other associated facilities. The City has approximately 34 miles of storm drain pipes, over 13000 basins and hundreds of culverts.

The City provides for the repair and replacement of the City's storm drain system and improvements to the storm drain facilities throughout the City on an ongoing basis. However, the City is proposing as part of the 2021-2025 CIP to develop a Storm Drain Master Plan (SDMP) that would include a comprehensive analysis for stormwater capture infrastructure, drainage areas, soil characteristics, and wellhead protection zones. Presently, the City relies on a complaint-driven process for storm drain repairs instead of a systematic program of preventative maintenance. The SDMP would serve as a planning guide for locating and sizing stormwater and drainage facilities. Adoption of a SDMP will assist in the self-reliance on the City's water supply and the NPDES compliance.

Developments within the project area would be required to adhere to applicable local, state, and federal regulations and standards, as well as implement site design measures, low-impact development, and best management practices

(BMPs), including infiltration features that contribute to groundwater recharge and minimize stormwater runoff, erosion, siltation, and/or flooding. The City is one of the permittees under the Los Angeles National Pollution Discharge Elimination System (NPDES) municipal storm water permit which means that any new development in the Plan area is subject to the Los Angeles Standard Urban Storm Water Mitigation Plan (SUSMP). The SUSMP addresses post-construction storm water pollution from new development projects.

7.4.6 ELECTRIC SYSTEM

PWP provides electric services in the Plan area with an energy system consisting of 1658 linear miles of overhead and underground power line, 11,163 poles, and 11 substations.³ The City owns and operates the Glenarm Power Plant that includes two power generating facilities. The system meets the City's power demand with 10 percent coming from PWP-owned generating facilities and the rest purchased from varied sources, both conventional and renewable, or through the wholesale energy market.⁴ Electrical infrastructure in the Plan area is located above ground on utility poles as well as below ground.

The Power Integrated Resources Plan (PIRP) is the PWP's guiding document for achieving internal power supply goals while upholding local, state, and federal mandates. The state requires that the PIRP be updated on a regular basis in conformance with the California Energy Commission regulations. The PIRP speaks to the City's commitment to shift the energy supply portfolio to low-carbon and renewable resources as well as exceeding state mandates for Renewable Portfolio Standard increase and greenhouse gas emissions reduction targets.⁵ The City also has an adopted Climate Action Plan that continues efforts to promote energy efficiency and reduce the City's dependency on traditional energy sources.

New developments in the Plan area would be required to comply with the California Energy Code, Part 6 of the California Building Standards Code (Title 24), CALGreen standards, the City's Green Action Plan, and the City's Green Building Standards Code, which collectively would increase efficiency and decrease consumption levels. Any new developments in the Plan area would require lateral connections to mainlines in coordination with existing utility service providers.

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³ City of Pasadena – CIP Electric System (2021) <https://www.cityofpasadena.net/public-works/wp-content/uploads/sites/29/14-Electric-Section.pdf>

⁴ City of Pasadena – PWP 'Where Our Power Comes From' Webpage <https://ww5.cityofpasadena.net/water-and-power/power/>

⁵ City of Pasadena – Power Integrated Resources Plan (2018) <https://ww5.cityofpasadena.net/water-and-power/wp-content/uploads/sites/54/2018/12/Pasadena-Water-and-Power-2018-IRP-Final.pdf>

7.5 Administration

7.5.1 GENERAL

The Specific Plan serves as the implementation tool for the General Plan and establishes the zoning regulations for the Specific Plan area. All development proposals within the Specific Plan area are subject to the procedures established herein, in addition to those procedures identified in Zoning Code Chapter 17.60.

The regulations and design guidelines in this Specific Plan subject to the Zoning Code will not become effective until the Zoning Code amendment process (by ordinance) is complete. Wherever the provisions and development standards contained in the Specific Plan conflict with those contained in the Zoning Code, the provisions of the Specific Plan shall take precedence. Where the Specific Plan is silent, the Planning Director or Zoning Administrator will interpret.

7.5.2 AUTHORITY

The City of Pasadena initiated and prepared the Lincoln Avenue Specific Plan pursuant to the provisions of California Government Code, Title 7, Division 1, Chapter 3, Article 8 (Sections 65450 through 65457). The law allows the preparation of specific plans as required for the implementation of the General Plan. Specific plans act as a bridge between the general plan and individual development proposals. They combine development standards and guidelines, capital improvement programs, and financing methods into a single document that is tailored to meet the needs of a specific area. Jurisdictions may adopt specific plans by resolution or ordinance.

The Lincoln Avenue Specific Plan is the regulatory document guiding land use and development within the boundaries identified in this Specific Plan. Upon adoption by ordinance, this Specific Plan will serve as zoning for the properties involved. It establishes the necessary plans, development standards, regulations, infrastructure requirements, design guidelines, and implementation programs on which subsequent project-related development activities are to be based. It is intended that local public works projects, design review plans, detailed site plans, grading and building permits, or any other action requiring ministerial or discretionary approval applicable to this area be consistent with this Specific Plan.

7.5.3 APPLICABILITY

All development proposals within the Specific Plan area are subject to those procedures identified in Chapters 17.10 and 17.40 of the Zoning Code.

7.5.4 INTERPRETATION, CONFLICT AND SEVERABILITY

A. Interpretation

In case of uncertainty or ambiguity to the meaning or intent of any provision of this Specific Plan, the Director of Planning and Community Development and/ or the Zoning Administrator have the authority to interpret the intent of the provision in a manner consistent with the goals, policies, purposes, and intent established in this Specific Plan. Refer to Chapter 17.12 of the Zoning Code.

The Director may, at his/her discretion, refer interpretations to the Planning Commission for consideration and action. Such a referral shall be accompanied by a written analysis of issues related to the interpretation. All interpretations made by the Director may be appealed to the Planning Commission in accordance with the appeal procedures in the Municipal Code.

B. Conflict

In the event of a conflict between the provisions of the Specific Plan and the provisions identified in the Municipal Code, the Specific Plan shall prevail. For any other topical issue, development standard or design guideline, and/ or regulation not addressed or otherwise specified in the Specific Plan, regulation and approval shall be carried out in accordance with the provisions of the Municipal Code, particularly Zoning Code Chapters 17.12 and 17.60. The particular section of code shall be based on the most appropriate or closely matching land use type or procedure, as determined by the Zoning Administrator.

C. Severability

If any section, subsection, sentence, clause, phrase, or portion of this Specific Plan, or any future amendments or additions, is for any reason held to be invalid or unconstitutional by the decision of any court or competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Specific Plan, or any future amendments or additions.

7.5.5 REVIEW AND APPROVAL PROCESS

All projects proposed within the Lincoln Avenue Specific Plan area shall substantially conform with the provisions of this Specific Plan. Chapter 17.60 of the Pasadena Zoning Code sets forth development review requirements and processes for approval of projects.

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A.1 Definitions

Amenity / curb zone: the portion of the sidewalk directly adjacent to the right-of-way, typically for street trees, parkways, street lights, street furniture, and bus shelters.

Blank wall: A windowless expanse of wall facing the street of 20 feet or greater.

Buildable area: The portion of parcel or development site which can be devoted to buildings and structures. This excludes slopes in excess of 50%, dedications, easements and other similar physical or legal constraints.

Building floor area: See ‘gross floor area’

Building frontage: The elevation of a building, measured horizontally at grade, that faces a street and includes the building wall which incorporates the main entrance(s) facing a parking area on the same premises. The main entrance to the building may include entrances to individual uses.

Building frontage zone: The portion of the sidewalk adjacent to a building (may be located in private property or within the right-of-way) that allows for door opening from buildings, outdoor seating, bicycle parking, and sidewalk seating.

Curb zone: See ‘Amenity / curb zone’

Façade: Any exterior wall plane of a building, ground level to top of roof

Fixed seating: Any seating that is permanently anchored to the ground or structure that cannot be relocated by an individual. Fixed seating may swivel or swing without being relocatable.

Floor area ratio: Numerical value obtained by dividing the above-ground area of a building or buildings located on a lot by the total area of the lot.

Footprint: The total ground floor area of the combined structures on a site or project area defined by the perimeter of the building(s). Includes parking structures; excludes parking lots and non-occupancy structures.

Frontage zone: See ‘building frontage zone’

Ground floor: The floor of a building or structure that is accessible from the finished grade or sidewalk

Gross floor area: Total enclosed area of all floors of a building measured to the inside face of the exterior walls including halls, stairways, elevator shafts at each floor level, service and mechanical equipment rooms and basement or attic areas having a height of more than seven feet, but excluding area used exclusively for parking or loading of vehicles or bicycles.

Ground floor space: An occupiable space on the ground level that can be directly accessed from the sidewalk within three feet of the sidewalk grade.

Height: Height is measured from the finished sidewalk existing grade of the site to an imaginary plane located above but parallel to the grade; projections and features such as appurtenances and railings per PMC 17.40.060.

Sidewalk line: A line parallel the property line accommodating the required sidewalk width as measured from the curb face.

Mixed-Use Project: Any building or project that combines more than one use type or housing type within a district, block, or parcel. It may also include housing built above commercial or commercial combined with light industrial uses.

Moveable seating: Any seating that is not permanently anchored to the ground or structure and can be relocated by an individual.

New construction: Completely new improvement of a property, or construction occurring on a property where the existing structures were complete razed. Does not apply to expansion of existing on-site improvements.

Open space: (Outdoor) space that serves a recreational function or provides visual relief from the building mass

Common open space: a usable open space shared among residents within a building or development; includes shared indoor and outdoor spaces unless otherwise defined. Subject to minimum dimension requirements established by the Specific Plan.

Public open space: a usable open space freely available to the public to use. Subject to minimum dimension requirements established by the Specific Plan.

Private open space: a usable open space adjoining and directly accessible to a dwelling unit, reserved for the exclusive use of residents of the dwelling unit and their guests. Subject to minimum dimension requirements established by the Specific Plan.

Usable open space: Unobstructed outdoor space that is landscaped and developed for active or passive recreational and leisure use, and is conveniently located and accessible; excludes required front yards not used for balconies or patios.

Parkway: (or landscaped parkway) landscaped or permeable areas located within or fully comprising the “Amenity / curb zone” between the building and the primary curb line.

Paseo: predominantly pedestrian-only open space at the ground level that is accessible to the public, that connects a right-of-way to another right-of-way, alley, or usable open space interior to a block; emergency vehicular access and commercial loading with limited hours are permitted. Subject to minimum dimension and design requirements established by the Specific Plan.

Plaza: usable open space at ground level accessible to the public. Subject to minimum dimension and design requirements established by the Specific Plan.

Primary curb line: the face of the predominant curb of an individual block forming the edge of the street.

Street frontage: the sidewalk line adjacent to the street.

Project: Refer to PMC 17.80.020

Projection: Anything attached to and extended outside the outer face of the exterior wall of a structure and not intended for shelter or occupancy. Examples include but are not limited to exterior stairs, fireplace, balconies, bay windows, lighting fixtures, parapets, shade structures, mechanical features, energy production panels or structures, etc.

Public realm: The publicly-accessible space between the street curb face and the building. This area includes the sidewalk zones and the required street setback, if applicable.

Residential Common Services: Those portions of a residential use building not dedicated to residential units that provide common services for residents. This may include spaces such as, but is not limited to, lobby or common building entry, leasing center, gyms/exercise space, shared kitchen, recreation center, screening or living room, business center, mail room, or library. These spaces/ portions of the building are permitted on the ground floor where residential units are not permitted.

Setback: The horizontal distance by which a development feature is required to be separated from the minimum sidewalk line. In some cases superseded by Setback range.

Street setback: The required setback from the minimum sidewalk line to the development feature. Not applicable to interior or rear property lines.

Setback range: minimum and maximum horizontal distances by which a development feature must be separated from the minimum sidewalk line or other property line. This measurement is similar to a “build-to” line.

Sidewalk zones: The three portions of a sidewalk that together comprise the public realm between a building and the street. Sidewalk zones are defined by the Pasadena Street Design Guide and regulated by the Specific Plan.

Shared property line: Any property line that exists to separate adjacent parcels or properties.

Stepback: Measurement of the required upper-story horizontal distance by which a development feature must be separated from the minimum sidewalk. Regulated as a horizontal distance above a defined vertical distance.

Streetwall: Any building façade that faces a street within 10 feet of the minimum sidewalk line.

Streetwall height: The portion of the street-facing building façade that rises from the sidewalk level to the required height without an additional setback or stepback. Required streetwall height is set by the Specific Plan for Zoning districts or specific locations.

Subterranean: The level of a building, inclusive of parking or habitable space, located primarily below the ground level with a top plate of two feet or less above sidewalk grade.

Transparent openings: Building openings (windows or doors) or transparent glazing that provide visual access into the structure.

Transparency: Provision of openings or surfaces that allow visual access into a structure.

Unbundled parking: Parking spaces, in any permitted configuration, rented or sold separately from a residential unit or non-residential square footage.

Walk zone: The portion of the sidewalk dedicated to pedestrian travel and free of obstruction.

Wrap-style development: Residential or commercial uses, at least 25 feet in depth, that wrap around an above-grade parking structure to screen structure from view from the street.

A.2 Design Guidance for Tree Selection

While the City of Pasadena Department of Public Works' Master Street Tree Plan ultimately determines what tree species is planted in public right-of-way, this appendix to the Lincoln Avenue Specific Plan is intended to guide discussions between the City and community when updating the Master Street Tree Plan for the area. During the Specific Plan update process, opportunities were identified to better align Lincoln Avenue's street trees with the vision, goals, and policies in the Plan related to shade, climate resilience, stormwater capture, and improved visual cohesion of the corridor. This appendix includes a description of the existing street trees along Lincoln Avenue within the Specific Plan area, followed by recommendations for potential new species.



Coast Live Oak at 1478 Lincoln Avenue



Holly Oak at 1270 Lincoln Avenue

A.2.1 EXISTING STREET TREES

The City's Master Street Tree Plan designates the following four street trees for the Lincoln Avenue Specific Plan area:

- » Coast Live Oak (*Quercus agrifolia*)
- » Chitalpa (*Chitalpa tashkentensis*)
- » Mexican Fan Palms (*Washingtonia robusta*)
- » Cassia Tree (*Cassia fistula*)

Estimations of current street tree inventory in this section are based on data from March 2021.

A. Coast Live, Holly, and Mesa Oaks (*Quercus spp.*)

There are currently approximately 31 oak trees in the Plan area, with roughly an even mix of Coast Live Oaks (*Q. agrifolia*) and Holly Oaks (*Q. ilex*), in addition to one Mesa Oak (*Q. engelmannii*). Coast Live Oaks and Mesa Oaks are both native to Southern California, while the Holly Oak is native to the western Mediterranean. These oak species feature dense, spreading evergreen canopies. Tree heights within the Plan area range from 11 to 45 feet but are most commonly between 25 and 35 feet along the corridor. Tree crown spread, often referred to as tree canopy, ranges from 20 to 65 feet and is most commonly about 30 feet. As native species, the Live Oak and Mesa Oak are well adapted to Pasadena's climate conditions and once established need minimal supplemental water. Historically, oaks have held significance throughout the development of Pasadena, and the Mesa Oak is sometimes referred as the Pasadena Oak.

Coast Live Oaks and other similar oak species are preferable for blocks with large front yard setbacks or single-story buildings which provide space to accommodate their wide canopy. Evergreen oak species are ideal for creating shaded pedestrian corridors.

B. Chitalpa (*Chitalpa tashkentensis*)

There are currently approximately 84 Chitalpa trees planted within the Specific Plan area. The Chitalpa tree is a hybrid of the Catalpa (*Catalpa bignonioides*) and Desert Willow (*Chilopsis linearis*) species and are characterized by long narrow leaves and abundant pink-white blooms during late spring and early summer. Tree heights range from 10 to 20 feet within the Plan area but are most commonly between 12 and 15 feet. Tree crown spread ranges between 5 and 20 feet and is most commonly about 15 feet. As a hybrid of two desert-adapted species, Chitalpas are incredibly drought tolerant and well adapted to streetscape conditions with minimal irrigation.

Chitalpa trees may have either a single trunk or multiple trunks, and both standard and multi-trunk forms can be found within the Plan area. The varied form, along with the low-branching habit, results in an informal character, which may not be considered desirable for street tree planting along a major street where a more formal structure is appropriate. The low-branching habitat produces limited shade compared to larger, more broad-canopy tree selections, and the deciduous foliage results in bare trees in the late fall and winter.



Chitalpa tree at 2016 Lincoln Boulevard illustrating a typical winter condition without leaves. This tree has a larger canopy relative to others along the corridor, though is still not ideal for producing shade.

C. Mexican Fan Palms (*Washingtonia robusta*)

There are currently approximately 82 Mexican fan palms planted within the Specific Plan area. Mexican fan palms are characterized by tall, narrow trunks topped with shiny, light green fronds. Tree heights range from 60 to 80 feet in the Plan area, with an average of 75 feet, and crown spread is consistently 15 feet. Mexican fan palms are well adapted to climate conditions in Southern California and require no supplemental irrigation once established. However, fan palms require frequent pruning to remove old fronds that are susceptible to being blown off by high winds, a risk to pedestrians and property below.

Mexican fan palms create a unified, vertical design element along Lincoln which can contribute to a sense of place and create visual landmarks for the avenue. However, the limited canopy span and immense height casts little meaningful shade and does not mitigate urban heat island effects.



Mexican fan palms along Lincoln Avenue

D. Crown of Gold (*Cassia fistula*)

Crown of Gold trees are designated for Lincoln Avenue, but have not been planted on the corridor to date. These trees have a dome-like canopy shape with deep green foliage and profuse yellow blooms in late spring. In street tree environments, tree heights are typically 20 to 30 feet and crown spread is typically about 25 to 30 feet. Crown of Gold trees are winter deciduous. Native to Southeast Asia, the Crown of Gold tree has a slightly tropical appearance but is tolerant of semi-arid conditions and once established needs minimal supplemental water.

When used as a street tree, they can create a stunning effect when blooming. As a deciduous tree, their use as a consistent street trees may not be appropriate for maximizing pedestrian shade objectives; however, limited use as seasonal accent trees may be appropriate for place-making purposes.



Crown of Gold Tree (none currently planted on corridor)

A.2.2 GUIDANCE FOR FUTURE TREE SELECTION

Trees play an important role in the experience of a streetscape. Through physical character, type of shade, and seasonal variety in the form of flowers or changing foliage, trees have a significant influence on our perception of a streetscape corridor. In addition to functional and placemaking selection criteria, tree species selection should follow urban forestry best practices and take into consideration resilience and future climate change impacts. The planting environment for street trees is harsh, with trees often being subjected to limited root zone volumes, minimal supplemental irrigation, pollution from car exhaust, pet waste, and high temperatures from urban heat island effect as well as light reflected from nearby glazing. As climate change continues, we can anticipate generally warmer temperatures and more extreme heat days; therefore, the role of shade trees in urban environments will become increasingly valuable and tree species should be suitable to warmer environments and extreme heat.

Table A.2-1 summarizes recommendations for tree species along Lincoln Avenue, which include both shade and accent trees:

- » **Dominant Shade Trees** provide shade for pedestrians and buildings, and would be planted consistently along the corridor.
- » **Seasonal Accent Trees** add seasonal interest and a splash of color to the streetscape experience, and would be planted at gateway intersections.

Accent trees are recommended to be planted in key gateway locations illustrated in Map A.2-1, which include the intersections of Lincoln Avenue and Montana, Wyoming, and Hammond Streets, Washington Boulevard, and adjacent to the 210 Freeway on- and off-ramps. In time, these trees will be a recognizable arrival point for pedestrians and drivers entering the Plan Area. The gateway tree should have a well-defined form and canopy shape, and a seasonal splash of color is strongly encouraged.

Although Mexican Fan Palms are well-adapted to local conditions and provide vertical character to the Lincoln Avenue streetscape, they are not recommended for planting as a street tree within the Specific Plan area moving forward because they do not meet the shade, stormwater capture, or climate objectives in the Plan or other citywide policy documents. As these palms reach the end of their lifespan, it is recommended that they be replaced by other species permitted by the Street Tree Master Plan.

Chitalpa trees are also not recommended for the Lincoln Avenue Specific Plan area moving forward because their informal character and limited shade potential do not meet the Plan objectives of creating an attractive, pedestrian-friendly streetscape environment that supports local business. Because of the prevalence of Chitalpa trees along the corridor, as funding becomes available the City may consider replacement of some of these trees in order to more quickly achieve consistent shade along the corridor, particularly when existing Chitalpa trees are adjacent to parcels that are unlikely to redevelop in the near future.

Table A.2-1: Tree Species Recommendations by Zoning district

Zoning District	Dominant Shade Tree Species	Seasonal Accent Tree Species
<ul style="list-style-type: none"> » Commercial General » Mixed-Use » Commercial Flex » Residential Multi-Family (south of the I-210 Freeway only) 	<ul style="list-style-type: none"> » Mesa Oak (<i>Quercus engelmannii</i>) » Chilean Mesquite - thornless variety (<i>Prosopis chilensis</i>) 	<ul style="list-style-type: none"> » Crown of Gold (<i>Cassia fistula</i>) » Golden Trumpet Tree (<i>Handroanthus chrysotrichus</i>) » Tipu Tree (<i>Tipuana tipu</i>)
<ul style="list-style-type: none"> » Residential Multi-Family (north of the I-210 Freeway only) 	<ul style="list-style-type: none"> » Coast Live Oak (<i>Quercus agrifolia</i>) » Mesa Oak (<i>Quercus engelmannii</i>) 	<ul style="list-style-type: none"> » Gold Medallion Tree (<i>Cassia leptophylla</i>)

A. Commercial General, Mixed-Use, Commercial Flex, and Residential Multi-family (south of the I-210 Freeway only) Zoning Districts

As the neighborhood develops, the Commercial General, Mixed-Use, and Commercial Flex zoning districts will share similar urban form characteristics in which buildings are placed within 0 to 10 feet of the sidewalk and building heights may be up to three stories. The area will be primarily comprised of commercial uses on the ground floor, including small retail, restaurant, and neighborhood services business that benefit from a cohesive sense of place and attractive streetscape environment. Therefore, it is recommended that street trees are retail-friendly and formal in their trunk and branch structure, and have canopies that can grow appropriately next to buildings with minimal setbacks.

Although the Residential Multi-family zoning district south of the I-210 Freeway does not share these urban form and use characteristics, it is directly across the street from the Commercial Flex zoning district, and therefore is recommended to be consistent in streetscape approach.

It is not recommended to require alternating tree species in these zoning districts given the infill nature of new street trees on Lincoln Avenue. Existing trees are not planted in a formal alternating pattern and therefore creating an alternative rhythm would be difficult to achieve. It would also not allow developers to choose a tree option that is most appropriate with their particular building frontage.

Dominant Shade Trees

- » Mesa Oak (*Quercus engelmannii*)
- » Chilean Mesquite - thornless variety (*Prosopis chilensis*)



Mesa Oak

For the dominant shade tree options, the use of native oaks is a consistent feature throughout the zoning districts and acts as the backbone tree type along Lincoln Avenue. Moving forward, the Mesa Oak may be more appropriate than the Coast Live Oak in these zoning districts due to their more columnar character that is less likely to conflict with facades at the property line. The more open canopy character is also more conducive to selective pruning if needed.

A second shade tree option, the Chilean Mesquite, is suggested within this area for variation in canopy color and texture, as well as the more dappled shade quality. This tree is more appropriate than oak varieties adjacent to 3+ story structures with little or no setback, as the tree canopy is less likely to conflict with building facades. This desert species is able to withstand extreme heat and is being used increasingly in southern California.

Seasonal Accent Trees (Gateway locations)

- » Crown of Gold (*Cassia fistula*)
- » Golden Trumpet Tree (*Handroanthus chrysotrichus*)
- » Tipu Tree (*Tipuana tipu*)
- » Gold Medallion Tree (*Cassia leptophylla*)

Accent trees options include the Crown of Gold tree, already designated for the Plan area, and other yellow-flower species such as the Golden Trumpet, Tipu, and Golden Medallion trees, which all have notable yellow flowers and a similar dome shape. The accent tree recommendations allow for some variety in species, but with an emphasis on a consistent flowering color to contribute to placemaking and the Plan area identity.



Chilean Mesquite

**B. Residential Multi-family Zoning District
(north of the I-210 Freeway only)**

The Residential Multi-family zoning district north of the I-210 Freeway includes two blocks of multi-family buildings characterized by large front yards that can accommodate street trees with large canopies. John Muir High School, on the west side of Lincoln Avenue, though outside the Plan area, has a large front yard with mature canopy trees, including oak species. For these reasons, the continued use of oaks, particularly the Coast Live Oak is recommended. The Mesa Oak is included here as an alternative shade tree where there may be spatial constraints, as the Mesa Oak tends to have a more columnar character. Consistent use of the oak species would have a striking visual effect and maximize shade for pedestrians and adjacent property owners.

Dominant Shade Trees

- » Coast Live Oak (*Quercus agrifolia*)
- » Mesa Oak (*Quercus engelmannii*)

Seasonal Accent Trees (Gateway locations)

- » Crown of Gold (*Cassia fistula*)
- » Golden Trumpet Tree (*Handroanthus chrysotrichus*)
- » Tipu Tree (*Tipuana tipu*)
- » Gold Medallion Tree (*Cassia leptophylla*)

Accent trees for this area are recommended to be the same species as those chosen for the remainder of the Plan in order to establish a consistent streetscape character along the corridor.



Crown of Gold



Golden Trumpet Tree

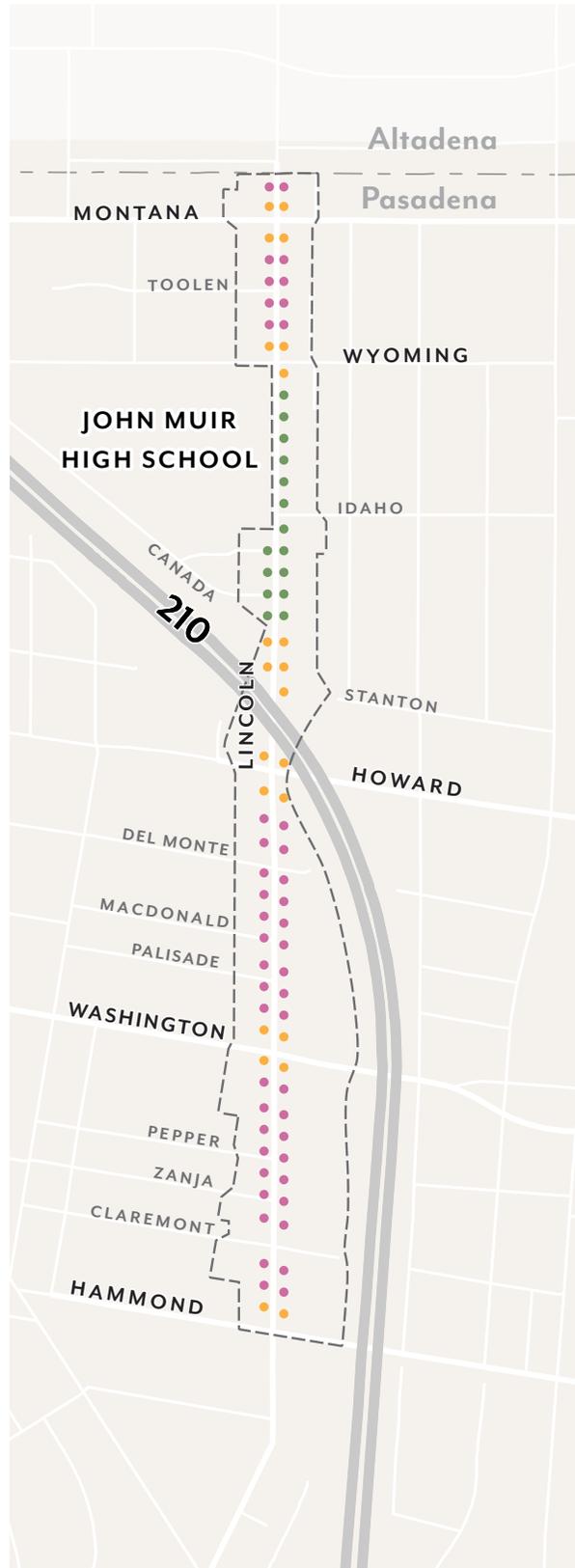


Tipu Tree



Gold Medallion Tree

Map A.2-1: Recommended Street Trees



- Specific Plan Area
- ● ● **Commercial General, Mixed-Use, Commercial Flex, Residential Multi-Family (South of I-210 freeway) Zoning Districts**
 Mesa Oak
 Chilean Mesquite
- ● ● **Residential Multi-Family (North of I-210 freeway) Zoning District**
 Coast Live Oak
 Mesa Oak
- ● ● **Gateway Intersections**
 Crown of Gold Tree
 Golden Trumpet Tree
 Tipu Tree
 Gold Medallion Tree

Note: Dots indicate recommended tree species, but are diagrammatic and do not reflect actual recommended tree spacing.

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